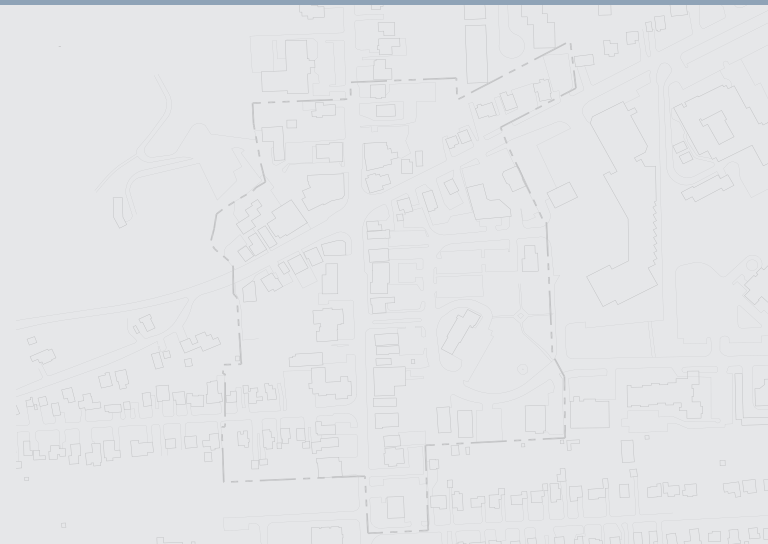


December 2014



Downtown Master Plan for Fenwick and Fonthill

Town of Pelham





acknowledgements

This study involved representatives and members of Pelham Beautification Committee, Town of Pelham Staff, Pelham's local historian, and other interested members of the public through stakeholder interviews and at a Design Workshop held in June 2009 and Open House held on May 8, 2014 in Fenwick and Fonthill.

At strategic points in the process, the Advisory Committee provided feedback, comments, ideas and concerns to help guide the study outcomes.

Amended in December 2014

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Downtown Master Plan

for Fenwick and Fonthill

Town of Pelham

Prepared by The Planning Partnership for the Town of Pelham

December 2014 as amended.



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1.0 introduction

1.1 Purpose of the Study

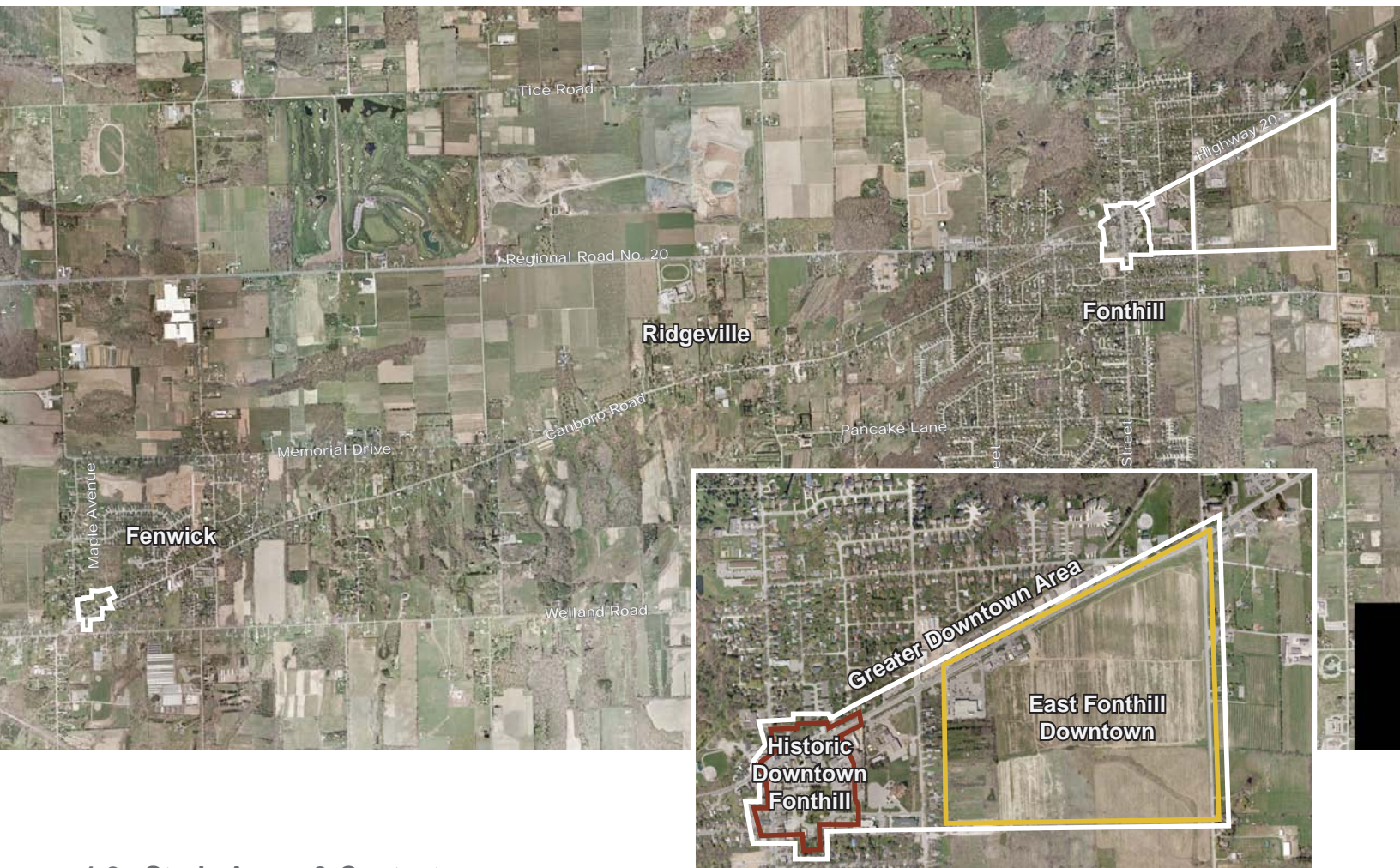
Change is occurring in the downtowns of Fenwick and Fonthill. This includes recent initiatives for improvements to Peace Park and streetscaping on Pelham Street in Fonthill and Canboro Road in Fenwick, along with new mixed-use development in the Commercial/Employment Centre in East Fonthill. Recognizing that these changes are taking place, the Town of Pelham has embarked on processes to discuss the future of these areas and to provide guidance for change.

In the winter of 2009, The Planning Partnership was retained by the Town of Pelham to undertake the Downtown Design Guidelines for Fenwick and Fonthill. An update to the document, now known as the Downtown Master Plan for Fenwick and Fonthill, was undertaken in the spring of 2014. This design initiative intended to address the challenge of transforming Pelham from a point of transit with no clearly defined image to a destination place where existing residents and visitors could benefit as a better place to live, work, and shop.

The purpose of the Downtown Master Plan for Fenwick and Fonthill is to provide an urban design vision and guidance for the Downtowns of Fenwick and Fonthill by addressing the nature, intensity and quality of development in both the public and private realms. This guideline document complements work already carried out in the Community Improvement Plan, and is intended to provide guidance for long-term development over the next five to ten years as the Downtowns aim to become distinct destination points for both existing residents and visitors.

The objective has been to think carefully about the street and buildings, conduct an analysis, and consult with the community in defining a vision for its future. With this in hand, the Town will be better prepared to respond to new development, as well as building restoration projects for existing businesses, occurring in the Downtowns with a clear and consistent rationale.

It is hoped that the analysis and direction identified through the course of this study will initiate a broader dialogue about growth and improvements to the Downtowns of Fenwick and Fonthill that are rooted in a clear understanding of what works, what is treasured and needs to be protected, and what ought to change.



1.2 Study Areas & Context

The Town of Pelham is located in the geographic centre of the Niagara Region. As identified in the Community Improvement Plan, the downtowns of Fenwick and Fonthill (Downtown Areas) are distinct planning areas. The two Downtowns are physically separated by a stretch of 6.5km, but are at the same time linked by Canboro Road, which also runs through the community of Ridgeville.

Modest in size, downtown Fenwick is approximately 2.67 hectares (6.6 acres), and is currently comprised of predominantly one- to two-storey house form buildings, including residences, mixed-use buildings, several automotive-oriented businesses, as well as institutional buildings such as the Maple Acre Library and the Fire Hall. Large single detached homes with landscaped front yards, manicured lawns and tree-lined streets predominantly surround downtown Fenwick.

In contrast to the village character of Fenwick, downtown Fonthill is approximately 9.33 hectares in size (23 acres) and has a wider range of land uses and a greater number

of businesses. Predominantly one- to two-storey mixed-use buildings with retail at-grade and single-use commercial buildings line the east side of Pelham Street, while employment buildings, a strip plaza, and institutional uses characterize the west side. Located within downtown Fonthill is also the Town Hall, Peace Park, and other public buildings including the library and Post Office. Marlene Stewart Streit Park is situated just outside the northwest boundaries of downtown Fonthill, while single family homes and newer townhouse developments surround the area. Other adjacent uses include a large commercial plaza, institutional uses and seniors apartments.

The update in 2014 served to incorporate the proposed commercial and mixed-use areas of the East Fonthill Secondary Plan and the north side of Highway 20, directly to the east of Historic Downtown Fonthill. Together these lands form the 'Greater Downtown' Area. Notwithstanding the general inclusion of East Fonthill in this Plan, a separate, and much more detailed Master Plan for East Fonthill has been prepared under separate cover.

1.3 Study Process

As part of the Master Plan study process, previously completed Town reports and Regional documents were reviewed, including:

- Community Improvement Plans for Downtown Fonthill & Fenwick (Draft), Town of Pelham (2009)
- Partners in Economic Growth and Prosperity: A Business Case for the Shared Investment in the Revitalization of Pelham's Downtown Cores, Town of Pelham (2008)
- Historically Significant Buildings of Pelham, The Pelham Historical Society (August 2007)
- Corporate Strategic Plan, Toward 2015, Town of Pelham (February 2007)
- Model Urban Design Guidelines, Regional Municipality of Niagara (April 2005)
- Urban Core Beautification Strategy, Town of Pelham (October 2002)
- Smart Growth in Niagara, Regional Municipality of Niagara (September 2001)

Clearly, an extensive amount of work has gone into studying the Downtown Areas, from a region-wide level to a more detailed local perspective. The Downtown Master Plan builds upon these previous studies, and was undertaken through a collaborative three-phased process which involved landowners and business owners, residential property owners, members of the Beautification Committee, the Town local historian, Town staff, as well as local political representatives.

The update in 2014 further considers initiatives that were undertaken since the 2009 process. These initiatives included:

- Canboro Street EA and Improvements (2014)
- Peace Park Improvements Plan (2013)
- Pelham Street Improvements (2010)

1.4 Report Structure

This guideline document is organized into the following sections:

Section 1.0 - Introduction

This section of the report provides background information and highlights the purpose of the study for the Downtown Areas.

Section 2.0 - Key Downtown Strategies

This section of the report outlines the overall vision for the Downtown Areas as defined in previous studies and further developed during the design workshop, summarized into eight key strategies.

Section 3.0 - Public Realm Framework

The Public Realm Framework provides guidance for elements in the Downtown Areas that are in public ownership - including streets and open spaces. The Public Realm Framework includes guidance for elements such as street character, open spaces, and recommendations for ensuring safely designed spaces.

Section 4.0 - Built Form Framework

The Built Form Framework provides direction for the appropriate use, scale and forms of development that is consistent with the overall character and vision for the Downtown Areas.

Section 5.0 - General Downtown Design Guidelines - Historic Fenwick and Fonthill

The General Downtown Design Guidelines focus on character-defining aspects of both new developments and the restoration of existing buildings such as storefronts, parking, building height, and material and design quality, in addition to environmentally sustainable design guidelines.

Section 6.0 - Demonstration Concept Plans

The demonstration concept plans for Fenwick, Fonthill and East Fonthill and before/after photomontages for Fenwick and Fonthill illustrate how the key downtown strategies can be achieved by applying the frameworks and guidelines to the Downtown Areas.

Section 7.0 - Implementation Strategies

The final section of this report provides strategies for implementation.

2.0 key downtown strategies

The Master Plan provides a framework for shaping the physical form, relationships and character of the Downtown Areas of Fenwick and Fonthill. The vision and principles defined in previous studies were drawn upon in helping to guide and shape the public realm and built form framework plans, but were further developed over the course of the study through the feedback received from key stakeholders and residents at the design workshop.

Consequently, the big ideas were refined and evolved into the key strategies for the Downtown Areas, which are summarized on the following pages. They remain current and relevant at the time of the 2014 update with the addition of an eighth strategy.

strategy one

Strengthen the “village” character of Fenwick and the “town” character of Fonthill.

Fenwick should promote its “village character” by maintaining its smaller one- to two-storey “house-form” buildings which are set further apart from each other; encourage adequate building setbacks to permit landscaped areas and/or patio space; and, promote local specialized businesses to attract local residents and visitors.



Fonthill should encourage its larger, more urban “town” character by a promoting a mix of mixed-use buildings, larger employment and civic buildings; set buildings closer to the sidewalk in order to create a more prominent streetwall; encourage a greater range of business types and civic presence; promote local activities and festivities; and, create linkages to adjacent attractions like Marlene Stewart Streit Park.



strategy two

Design streets for people.

Streets play an important role in the urban environment. They should be designed with the pedestrian in mind, and not just as a way for cars to move around. In order to encourage lively streetscapes and extend activity and use into the sidewalks, streets in both Downtown Areas should be designed with the following in mind:

- narrow the roadway and widen sidewalks
- create on-street parking buffer zones
- create opportunities for places to sit and gather, such as cafés and patios
- provide trees and landscaping along the boulevards
- design to human scale
- consolidate access points and use of rear lanes
- design raised or articulated crosswalks
- in Fonthill, find alternate truck routes to Pelham Road, such as Rice Rd, Port Robinson Rd and/ or Quaker Rd.



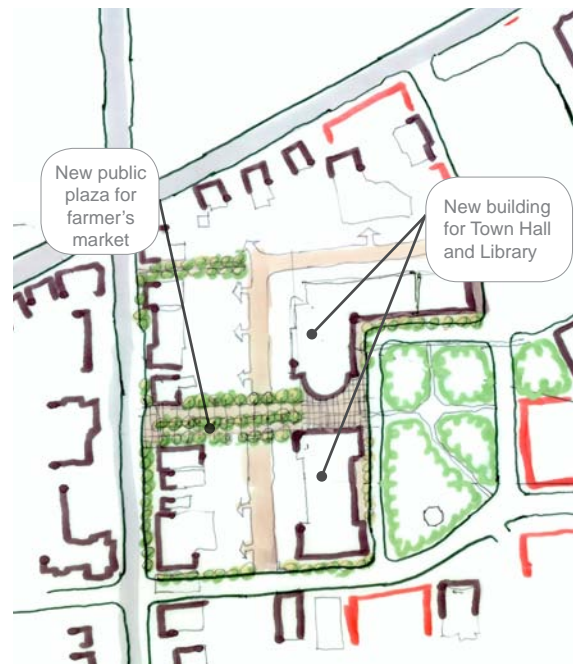
strategy three

Complete Pelham Town Square.

Pelham Town Square should be re-designed to accomplish the following design goals:

- complete the street network
- create more pedestrian connections
- conceal the backs of buildings and parking areas
- define and frame open spaces
- create a permanent farmer's market site
- find a strategic location and design for a new consolidated Town Hall & Library
- promote a civic presence and create a "wow" factor

Pelham Town Square re-designed conceptual options



Current building footprint, block configuration and street system of Fonthill

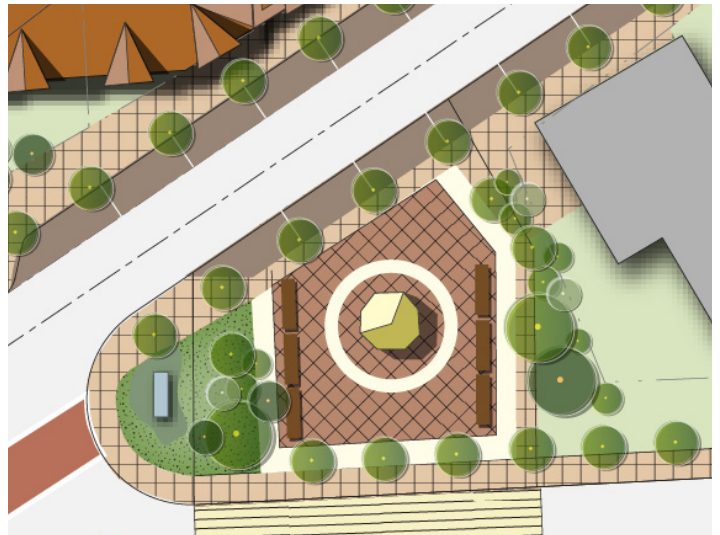


strategy four

Create a focal point for Fenwick.

Fenwick should have an identifiable local focal point which:

- is in a central-highly visible location
- acts as a new gathering place for residents and visitors
- has a distinct-high quality design
- is framed by vibrant uses

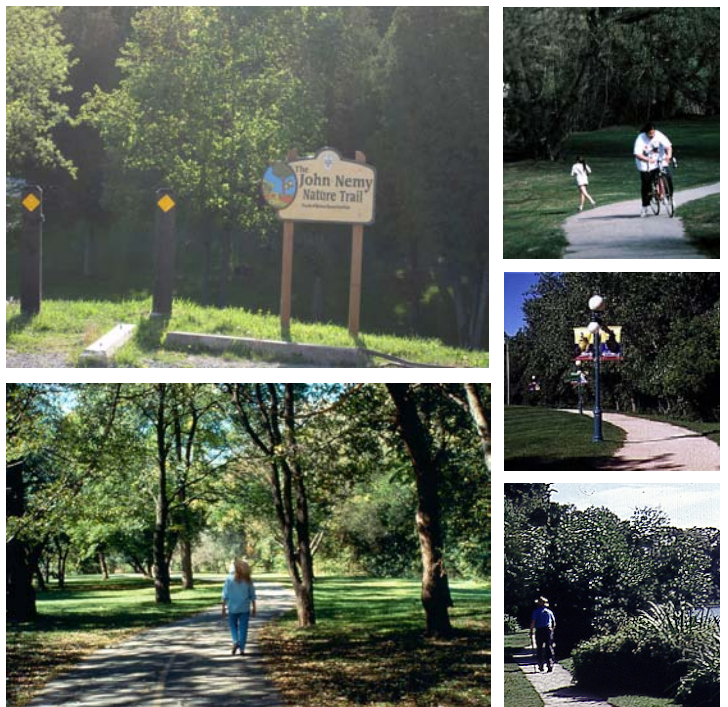


strategy five

Strengthen cross-town and neighbourhood connections.

In order to enhance connectivity:

- Downtown Areas need to be pedestrian and bike destinations, not just through-routes
- create midblock pedestrian connections
- create landmarks
- complete missing links and enhance the Steve Bauer Trail
- create a network of existing and new open space linkages
- establish a consistent family of signage including wayfinding elements
- establish a recognizable cultural heritage circuit



strategy six

Rediscover the cultural heritage of Pelham.

The Downtown Areas are rich in **cultural heritage** and should be preserved and enhanced through the:

- identification of significant heritage assets
- development of standards for building rehabilitation and site redevelopment
- development of a program for highlighting heritage resources

Historic and current photographs of the A.B. Damude building (Currently Ho Ho Chinese Food and Zenkai Massage Therapy Clinic), located at 8 Highway 20 in Fonthill.



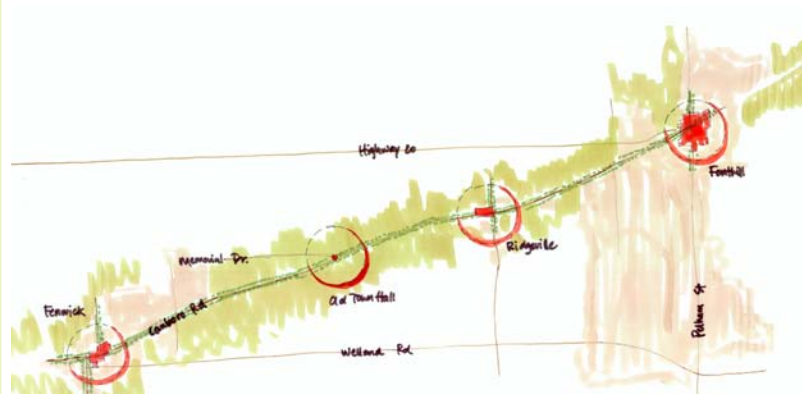
Historic and current images of Fenwick Railroad Museum, 1411 Maple Street, Fenwick. This frame building was built in 1807 at the corner of Effingham Road and Welland Avenue, and moved to Fenwick in 1929.

strategy seven

Promote a Pelham historic “promenade”.

The 6.5km stretch of **Canboro Road** acts as a main connector between the Downtowns of Fenwick and Fonthill. Consideration should be given to enhancing this stretch of road as a destination and cultural heritage experience through the:

- enhancement of Canboro Road as a scenic route connecting a series of destinations
- promotion of economic and tourism opportunities, for example, agri-tourism, along the route
- promotion of Canboro Road through public events (ie. bicycle competitions, running marathons, food-tasting events, etc.)
- potential for historical interpretative trails



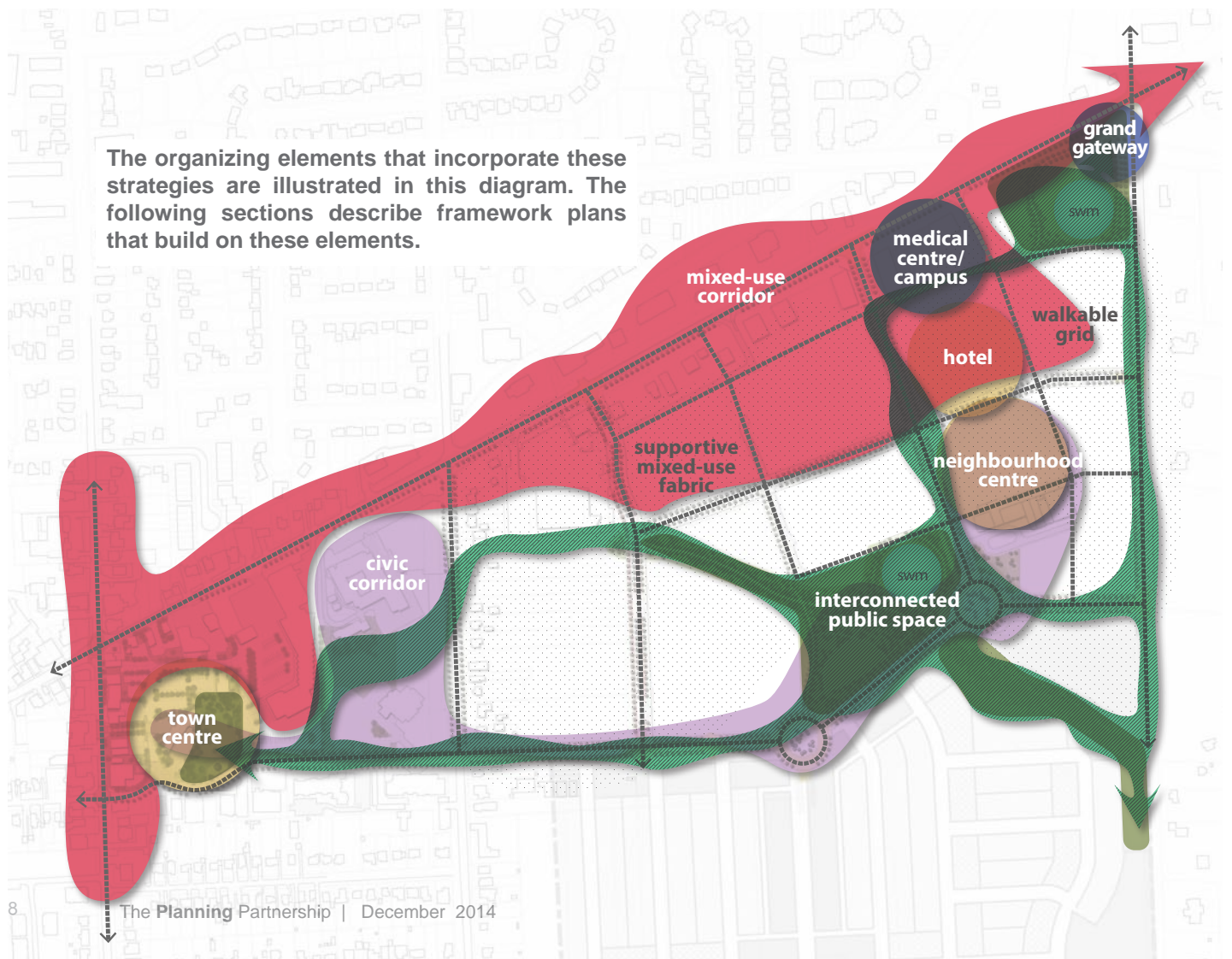
strategy eight

A Greater Downtown Fonthill

The proposed commercial and mixed-use area in the East Fonthill Secondary Plan presents an opportunity to create a 'Greater Downtown Fonthill' strategy that connects and integrates the historic downtown area with the new development to the east. In doing so, both older and newer areas can thrive by retaining their distinct but complementary built characteristics. The Greater Downtown Fonthill strategy is based on:

- Ensuring strong and seamless physical connections through existing and new streets, open spaces and trails
- Reinforcing visual connections with continuity in street-oriented built form and consistent streetscaping, as well as views to gateways, public art and other landmarks across both areas
- Ensuring new uses and developments reinforce the distinct qualities of the old and new areas, and not compete with the fine-grained and small-scaled character of uses in the historic downtown area
- Protecting Highway 20 to serve as the east-west 'mixed-use corridor', while maintaining the extension of Pelham Town Square to Rice Road as the 'civic corridor'

The organizing elements that incorporate these strategies are illustrated in this diagram. The following sections describe framework plans that build on these elements.



3.0 public realm framework

3.1 Public Realm Framework Plan

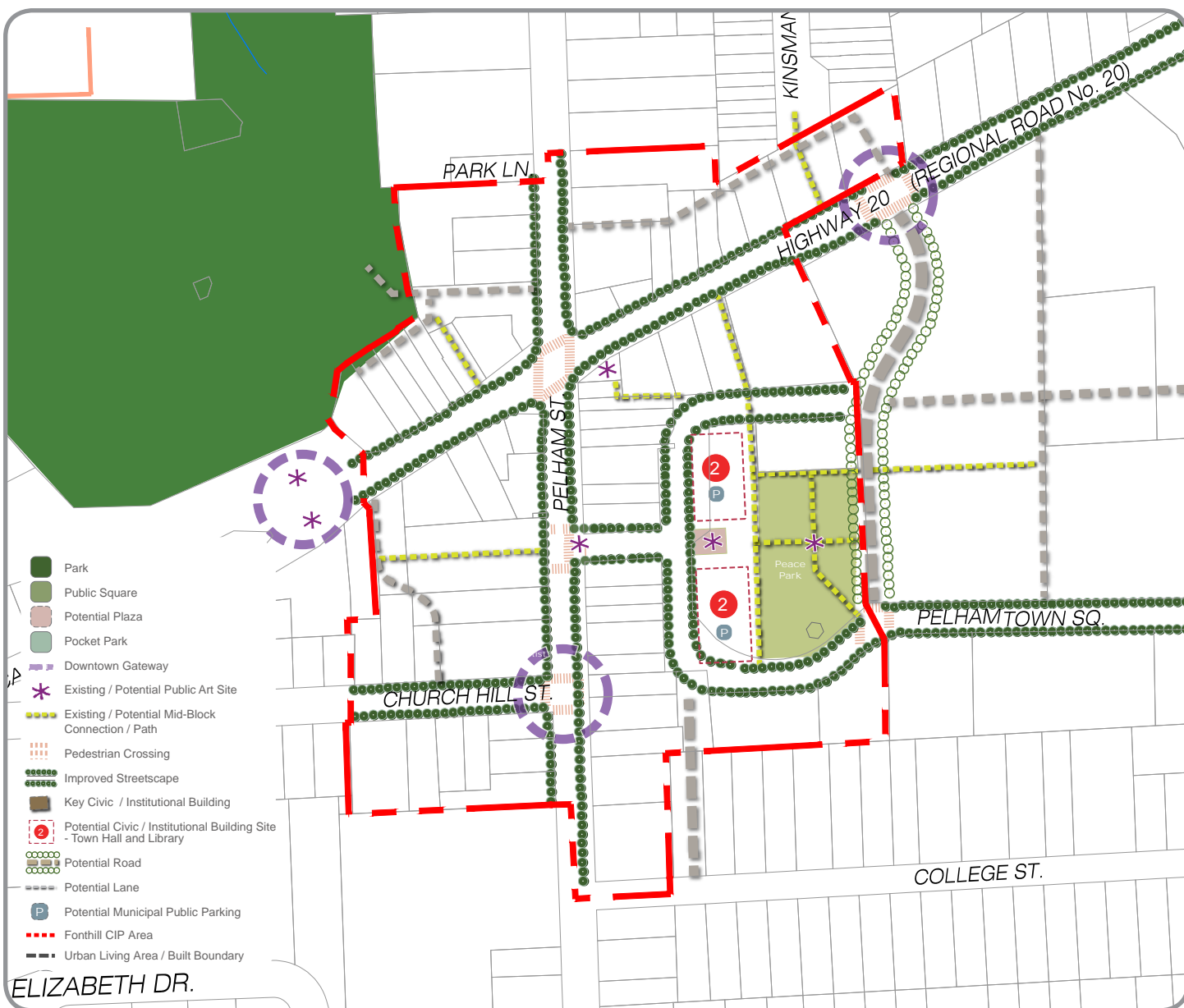
The Public Realm Framework Plan defines the desired future components, character and relationships that comprise the public domain including streets, parks and areas/sites for civic uses. The Framework is central to the vision because it shapes the most prominent and visible aspects of the built environment and thus makes the greatest impact on the image of both Downtowns. The intent of the Public Realm

Fenwick Public Realm Framework Plan



Framework is to direct and coordinate public investment decisions so as to cultivate and nurture a legible, coherent and appealing physical setting. Enhancing the pedestrian experience and providing for a variety of civic spaces can strengthen the area's liveability and economic vitality. The components of the Framework and applicable guidelines are described on the following pages.

Historic Downtown Fonthill
Public Realm Framework Plan



3.2 Potential New Streets and Lanes

While no new streets or lanes are proposed for Fenwick, the Public Realm Framework Plan for Fonthill identifies the potential for several new and re-aligned streets and lanes, in addition to closures, in order to create better access and circulation systems through the Downtown and Greater Downtown Area.

New Streets/Closures

Pelham Town Square

Section 2.0 of this report identifies that one of the key strategies is to “Complete Pelham Town Square”. As demonstrated in the Public Realm Framework Plan, there are three fundamental stages in achieving this : 1) Close the road that currently leads to the Town Hall building; 2) Extend Church Hill Street east so that it connects to current Pelham Town Square; and 3) Over the long-term and in conjunction with redevelopment, formalize a new street that connects Highway 20 to Pelham Town Square through the existing commercial plaza.

This re-design effectively alleviates traffic problems that leads in and out of the Town Hall occurring during peak hours. More importantly from a comprehensive view, this enables the completion of the block where the new Town Hall site and other civic functions can frame Peace Park to create a proper civic node, while concealing the rear of properties fronting onto Highway 20 and Pelham Street.

With the closure of the entry street to the Town Hall, this space can remain in public ownership as a formal plaza, and transformed into a more appealing and active space for public enjoyment - for example, to hold farmer’s market events.

By extending Church Hill Street, it should be noted that this strategy would require that the Town acquire one property along Pelham Street. Depending on the alignment, portions of other properties may also be necessary.

East Fonthill

A new street network is also proposed in conjunction with the development of East Fonthill. The intent over the long-term is to create a network of walkable scaled streets and blocks. However, in the interim larger blocks with internal private connections may be permitted to supply predominately large format retail development.

A key objective is to extend Pelham Town Square east of Station Street to Rice Road as a ‘civic corridor’ linking a series of existing and proposed institutional uses and key open spaces. The proposed roundabouts will also distinguish and reinforce the importance of this east-west connection between the Downtown and Greater Downtown Area.

Rear Lane System

In addition to the new road system proposed in the Public Realm Framework Plan, internal lane systems are introduced along the rear of properties to permit the reduction of the numerous driveways that currently exist along Pelham Street and Highway 20. Not only does introducing lanes reduce the number of pedestrian/vehicle conflicts, but it also enables a more safe and pleasant streetscape character, which would encourage people to walk more in the Downtown Area.

3.3 Parks

While Fenwick does not have any significant parks, Fonthill is afforded with a number of green open spaces of differing scales and function. Immediately adjacent the study area is Marlene Stewart Streit Park - a large open space with a baseball diamond, playground, outdoor pool, skate park, passive use areas, and trail linkages to the John Nemy Nature Trail. New parks are proposed for East Fonthill that integrate proposed stormwater management facilities. As the Greater Downtown Fonthill develops there are opportunities to enhance the visibility of these existing and new open spaces and improve physical linkages through the following guidelines:

- Enhance visual and physical connectivity to existing parks through streetscapes, pedestrian connections and signage.
- Open spaces that serve a broad population, like Marlene Stewart Streit Park, should continue to serve both passive and active recreational opportunities for all ages.
- The design and location of open spaces should utilize Crime Prevention through Environmental Design (CPTED) principles by ensuring:
 - Clear views in and to surrounding areas;
 - Adequate lighting;
 - Fronting buildings to overlook public space;
 - The use of signs and design for ease of access and egress; and,
 - A mix of activity for constant use of the space.



Marlene Stewart Streit Park - Skate Park

3.4 Public Squares

In Fonthill, Peace Park is the focus of the community. Located adjacent to the Town Hall, the square is a formal gathering space and is currently well-used by residents. The main gazebo is a popular social spot where concerts and festivals often take place.

In response to the growing popularity of Peace Park as a gathering place for major events and festivals, a plan for redesign and improvements was undertaken in 2013. Among other things, the design proposes an outdoor amphitheater, public washrooms and the introduction of enhanced accessibility features. It also anticipates and enables the potential future transformation of the entire block, as envisioned by this plan.

A new potential public square is proposed in conjunction with the development of the East Fonthill area. This square is intended to provide a potential focus and gathering place for the Greater Downtown Fonthill area, and will serve as a second and east open space anchor for the Downtown.

Where new development is proposed around these squares, there are opportunities to enhance their status through the following guidelines:

- Locate features such as public art to visually enhance and connect the square to other open spaces.
- Front buildings to frame and overlook public spaces.
- Buildings that front onto open spaces should include complementary uses that can animate these spaces in all hours and seasons, such as cafés and restaurants.
- Utilize signage for ease of access and egress.
- Provide adequate lighting for safety.
- Formalize paths and sidewalks consistent with pedestrian desire lines.



Gazebo in Peace Park along Pelham Town Square

3.5 Plazas

A plaza is generally a paved open space often associated with a civic or commercial function. They vary in shape and size depending on their purpose, but are generally smaller and more intimate in scale than parks or squares. Plazas generally serve high pedestrian traffic areas and/or where major events need to be accommodated that generate large crowds. Year-round plazas function best when they are framed by highly animated uses such as shops, restaurants and cafés.

In the case of Fenwick and Fonthill, there is currently no formal public plaza. A centrally located public plaza can serve as a focal point for residents to gather for special events and concerts, which currently does not exist in Fenwick. Introducing a formal plaza space in Fonthill would not only allow for a vibrant and attractive pedestrianized space for the already established farmer's market, but could also encourage other social activities to take place, such as festivals and musical performances.

Initially a temporary structure, the current archways over the connecting street between Pelham Street and Pelham Town Square begin to celebrate and formalize this potential plaza space.

The Public Realm Framework Plan identifies a number of opportunities for new plazas to reinforce the prominence of certain civic or commercial uses as well as for accommodating occasional events. Guidelines include:

- To ensure utilization and presence in the downtown areas, the plaza should be fronted by animated uses with a high level of transparency. These would be high-traffic areas such as restaurants and cafés, preferably with some outdoor seating areas.
- Utilize distinctive, high quality paving treatments.
- Extending the paving treatment onto the street can give the space further prominence with delineating an extended space that can be occasionally utilized for large-scale events through street closures.



Potential plaza space during community festival.

3.6 Pocket Parks

Within Downtown Fonthill, a pocket park is located at the south-east corner of Pelham Street and Highway 20. Despite this exposure, this park is poorly framed on either side by blank walls. In order to enhance the visibility and connectivity of this and the other pocket parks, guidelines include:

- High quality landscaping should be used.
- Seating should be provided to encourage activity and use.
- Public art should be located at this prominent corner (See Section 3.7 *Potential Public Art/Landmark Sites*), and designed such that it does not block the safety sight lines of the intersection.
- Adjacent buildings that front the pocket park should be encouraged to provide transparency and access.

3.7 Potential Public Art / Landmark Sites

Public Art provides an opportunity to celebrate and showcase local arts and culture, and can help establish a unique identity and enhance the quality of the public realm in ways that conventional streetscape elements cannot. Key locations for public art have been identified for particular visible sites including open spaces, gateways, and at the terminus of view corridors.

Where public art sites are identified, they should follow these guidelines:

- Public art may include memorials, sculpture, water features, murals, or individual art installations at visually prominent sites, open spaces, gateways, along sidewalks and in association with public buildings.
- Public art sites should be highly visible and as such, these sites should include installations that serve as accents to the downtowns, as orienting devices for moving about, or as focal points in public open spaces.
- The scale of the installation should correspond to the visual prominence of its site.



Small parkette on corner of Highway 20 and Pelham Street



3.8 Gateways

The Public Realm Framework Plan identifies potential Gateways to Fenwick and Fonthill. Clearly defining Gateway areas helps to enhance orientation, lends to a sense of place and can generate civic pride. Gateway sites help signal key points of entry into the Downtown Areas. These locations provide key opportunities where the coordinating of the design of landscapes, signage, public art and buildings can create a sense of entry and orientation. The expression of a Gateway can take on many forms and will hinge on the individual circumstances of the site.

The Framework Plan encourages the creation of Gateways according to the following design objectives:

- Gateway experiences should reflect the unique culture and history of the downtowns.
- Gateways should be well integrated while marking a sense of entry for visitors.
- The built form and public realm context of the Gateway should be held to higher design standards.
- Gateways should exhibit coordinated site planning, streetscaping, built form and landscaping, to create a unified environment. This can be achieved through:
 - celebrating Gateways with public gathering spaces;
 - locating primary building entrances at Gateways;
 - providing visually prominent massing, such as distinct corner or roof treatments;
 - special attention to architectural and material quality;
 - locating public art at Gateways;
 - consistency of materials, colours and textures in built form and landscape (for example in building façades and paving materials);
 - providing special streetscape elements or furnishing such as signs, arches, columns, or fountains;
 - consideration for visibility at night and winter months through lighting and vertical expressions; and,
 - ensuring that parking, loading, servicing, utilities, mechanical equipment are located out of view.





3.9 Mid-Block Connections & Paths

In order to encourage walkability within the Downtowns and improve circulation and access, mid-block connections are crucial design elements that allow flexibility, giving pedestrians and cyclists choice to get to their destination points in sometimes the most direct route. Consequently, they can occur either in the private realm (for example, between two buildings), through semi-private spaces such as covered building arcades, or as pathways through public spaces.

Especially in Fonthill where blocks are larger and there are multiple destination points, new and existing connections and paths will enhance the connectivity of the area to the surrounding neighbourhoods. Several strategic locations have been identified in the Public Realm Framework Plan. The design of these connections are important and should consider the following guidelines:

- Paths should provide for safe street crossings to connect to other open spaces and civic destinations.
- Paths should be designed with adequate lighting and furnishings for use in all seasons and hours.
- Where possible, paths should be designed to accommodate cycling.
- Where possible, grade-level uses should animate these paths and connections.

3.10 Pedestrian Crossings

Heavy truck traffic, speed, road intersection angles creating blind spots, long crossing distances, and undefined pedestrian crossings were some concerns identified in this study.

To help with these issues, the Public Realm Framework Plan identifies existing and potential crosswalks in strategic locations. These generally correspond to existing signalized and proposed intersections as well as key public destinations.

Crosswalks serve two functions: the clear demarcation of a safe route for a pedestrian to cross and as a traffic calming measure. Frequent stops will ensure slower traffic speeds and encourage cautious driving. The guidelines include:

- Where possible, every four-way intersection along a major pedestrian corridor (such as Pelham Street and Highway 20 in Fonthill, and Canboro Road in Fenwick) should have a crosswalk.
- To enhance their visibility and quality, crosswalks should utilize distinctive feature paving.
- Signalized pedestrian crosswalks should be provided at locations where important civic destinations and/or significant walking traffic is anticipated, such as at parks and plazas.





Shared Intersection located in Markham Village (Main Street)

3.11 Shared Intersection

Shared Intersections allow pedestrians and vehicles to share the same areas without the traditional hard distinctions such as curbs and sidewalks. These intersections are not typically paved with asphalt, rather they feature a variation in materials including decorative pavers to define the space. All modes of transportation are given equal priority on this type of intersection. The space is used by pedestrians and acts as a host for outdoor activities, becomes a destination and is more vibrant than a traditional street and intersection.

Examples of Shared Intersections include:

- Main Street Markham Village, Markham, Ontario
- Argyle Street, Chicago, Illinois
- Market Square, Pittsburgh, Pennsylvania



Shared Intersection located in Markham Village (Main Street)



Argyle Street, Chicago, Illinois



Market Square, Pittsburgh, Pennsylvania

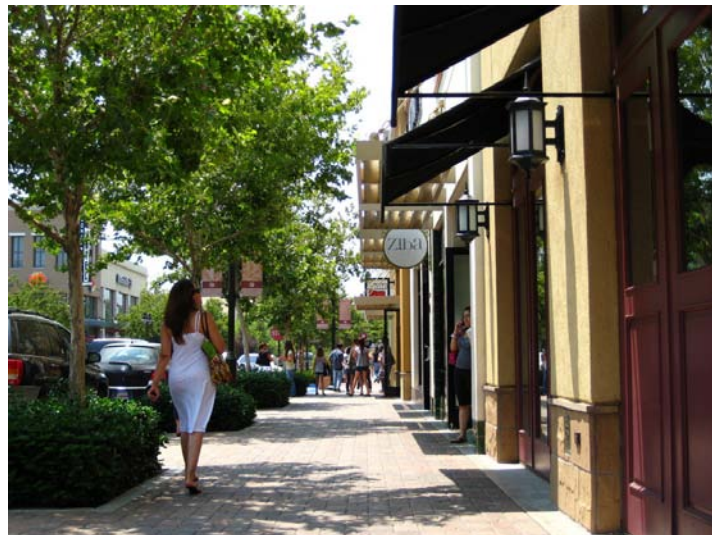
3.12 Improved Streetscapes

Fundamental to any commercial street is the quality of the pedestrian context as enabled by a splendid streetscape environment. Improving the use, character, and quality of streetscapes is one of the most significant means for enhancing the Downtown Areas.

As the streetscaping improvements completed on Pelham Street and underway in Fenwick demonstrate, coordinated streetscape design ideas ensure that a coherent vision is realized. General guidelines for the design of streetscapes include:

- Bury overhead telephone and hydro wires.
- Explore opportunities to ‘green’ the street through trees and landscaping.
- Explore ways to visually and physically extend the pedestrian realm through the broadening of sidewalks at local side streets, distinctive paving of the parking lanes, extending the plaza treatments over the roadway, and demarcating crosswalks.
- To avoid visual clutter and unnecessary obstructions on sidewalks, streetscape improvements should integrate lighting, furnishings, bicycle parking, newspaper boxes, trash bins, signage, etc. within a unified design vocabulary.
- Explore ways to incorporate public art and individual expression within a consistent and coherent framework that ties the area together.
- Consider a lighting strategy to improve pedestrian safety, but also consider how illumination can be utilized as a decorative feature to enhance the night experience.

Prototypical sections for improvements on other existing or proposed spaces in Greater Downtown Fonthill are illustrated on the following pages, and discussed in more detail in *Section 7: East Fonthill Public Realm Guidelines*.



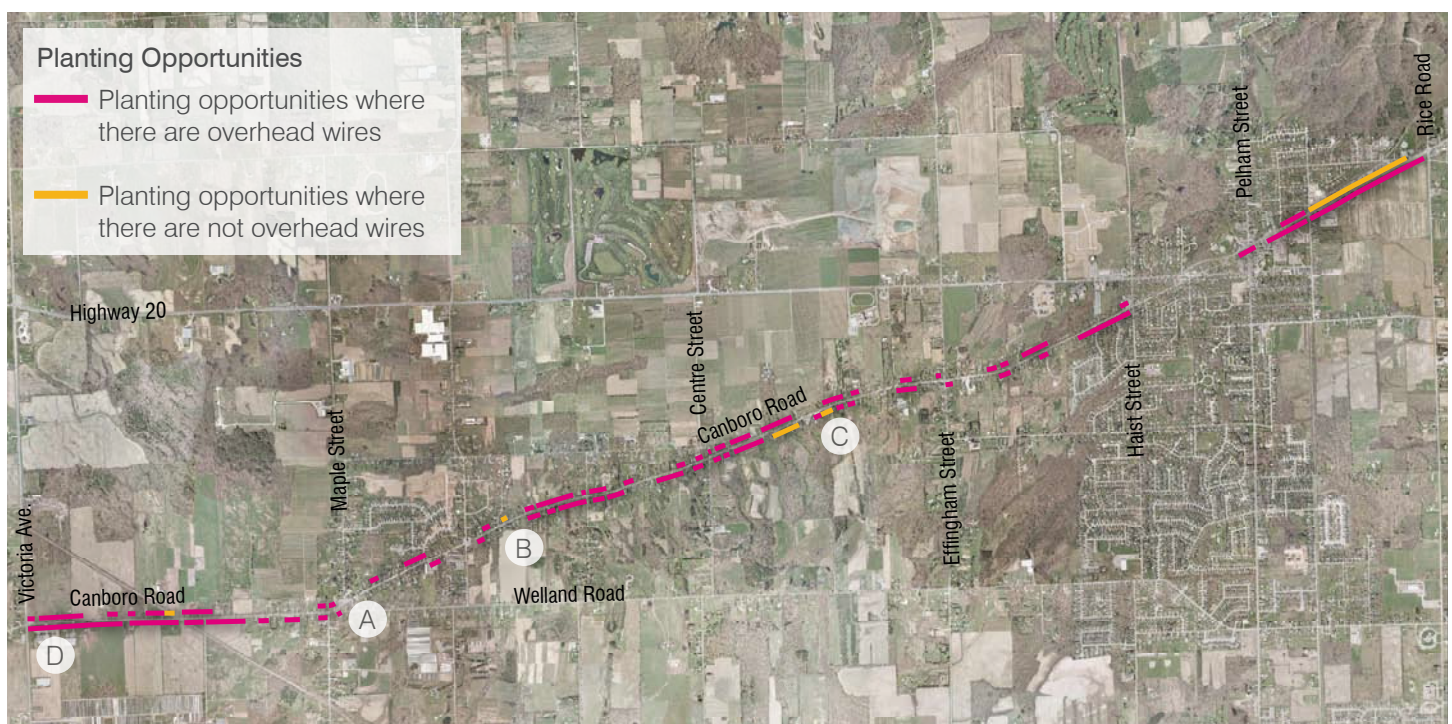
Proposed Enhancement Strategy for Canboro Road – Rural Component

Canboro Road is a crucial connector between Fenwick, Ridgeville and Fonthill. It has historical significance, and many heritage buildings and landscapes are located along this route. The road in the rural components has a typical rural cross-section. There are numerous large trees, but, over time, much of the historic tree canopy has been removed due to the installation of overhead wiring. The objective of this enhancement strategy is to:

- Create a new tree canopy that unifies this rural streetscape, and celebrates Canboro Road as a beautiful route that connects Fenwick, Ridgeville and Fonthill;
- Establish a 4-season landscape with this newly planted streetscape, showcasing amazing spring flowers and fall colour, and the best of the Niagara region trees; and,
- Celebrate the history of Pelham by establishing a roadside plugging program that identifies the inventory of historic buildings and sites along the route for the benefit of the travelling public - on foot, by bicycle and in cars.

The associated map of Canboro Road identifies a number of key locations where the new tree planting program could be implemented, and a tree species list that includes larger trees for locations without overhead wiring, and lower scale trees that could be planted below the wires are identified. Photographs show the existing condition, while associated photomontages show what the streetscape could look like through the suggested planting program. By developing a small collection of species complexes, species diversity can be exploited toward a more resilient landscape, and a more beautiful street, with strengthened connections between these communities of Pelham. The idea is to have trees that create a canopy, that will not interfere with overhead wiring, and that are colourful and beautiful throughout the year.

It is anticipated that the tree planting program could occur solely within the public road right-of-way, or on adjacent private or publicly owned properties or some combination of both. Nonetheless, it would be appropriate to work with adjacent property owners to determine willing hosts. This tree planting program could be carried out incrementally, over a number of years.



Tree Species List

The following tree species list identifies a collection of species complexes that include larger trees for locations without overhead wiring, and lower scale trees that could be planted below overhead wiring, respectively.

Complex One

- Tuliptree - *Liriodendron Tulipifera*
- Black Gum - *Nyssa Sylvania*



Tuliptree - *Liriodendron Tulipifera*



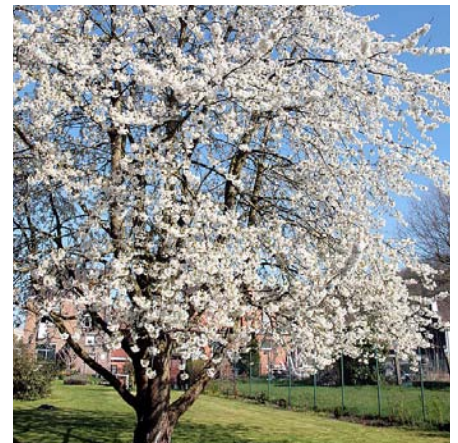
Black Gum - *Nyssa Sylvania*

Complex Two

- Black Maple - *Acer Nigrum*
- Common Serviceberry - *Amelanchier Canadensis*



Black Maple - *Acer Nigrum*



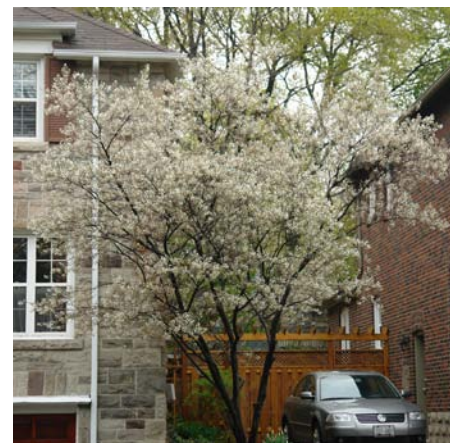
Common Serviceberry - *Amelanchier Canadensis*

Complex Three

- Silver Maple - *Acer saccharinum*
- Sweet Cherry - *Prunus Avium*



Silver Maple - *Acer saccharinum*



Sweet Cherry - *Prunus Avium*

A - Proposed Enhancement for Canboro Road - Rural Component

Before



After



B - Proposed Enhancement for Canboro Road - Rural Component

Before



After



C - Proposed Enhancement for Canboro Road - Rural Component

Before



After



D - Proposed Enhancement for Canboro Road - Rural Component

Before



After



3.13 Public Parking

Most of downtown Fenwick and Fonthill's parking area is currently accommodated on-street and in surface parking lots. The majority of individual businesses provide their own parking areas, either at the front or at the rear of their property. While public buildings attempt to internalize surface lots, driveways are wide and they are not sufficiently landscaped. Except for Pelham Street and eventually Canboro Road in Fenwick, on-street parking areas are mostly asphalt and are not clearly demarcated from the roadway.

A review of the parking capacity in the Downtown Areas in 2009 revealed that the existing parking supply in Fonthill is approximately 720 spaces, with an additional 550 spaces supplied in the adjacent St. Alexander Catholic Elementary School, St. Alexander Roman Catholic Church, and the commercial plaza. The available parking supply is very satisfactory to serve uses, with over 1,200 parking spaces within a 5-minute walk.

In Fenwick, the current parking supply is comprised of approximately 95 off-street parking spaces, 23 on-street parking spaces along Canboro Road, and 20 parking spaces at the Fire Hall. An additional 25 on-street parking spaces could be accommodated along Welland Road. Like Fonthill, Fenwick is well served by the amount of parking available.

On-Street Parking

As with most downtowns, retail relies on the availability and proximity to parking. On-street parking not only provides this, but it also functions as a traffic calming device to slow traffic and acts as a safety buffer between pedestrians and vehicles. Where possible, on-street parking should be provided on one or both sides of downtown streets. On-street parking areas should be demarcated with a special pavement treatment in order to distinguish parking lane from the roadway.

Public Parking Lots

A key objective is to promote the walkability of the downtowns; however, it is crucial to recognize that Fenwick and Fonthill will also be accessed and serviced by vehicles. To this end, how parking is accessed and where public parking is located will be an important design consideration. As the area redevelops, key guidelines for public parking should include:

- No parking should be allowed at the front of buildings along Pelham Street and Highway 20 in Historic Downtown Fonthill, and along Canboro Road in Fenwick; all parking should be accommodated either on-street or in parking areas at the rear of buildings.
- Where possible, rear lanes should be used in order to minimize the number of driveways along the street.
- Local business property owners should consider consolidating rear parking areas to create shared/integrated parking lots. This will increase efficiency and the number of parking spots, in addition to minimizing the number of access points which create pedestrian/vehicular conflicts.
- Any parking areas visible from the street should be landscaped in order to minimize its presence.
- Municipal public parking should be integrated into the potential development of a new Town Hall & Library, as well as any other future community facilities.



Clearly demarcated on-street parking areas

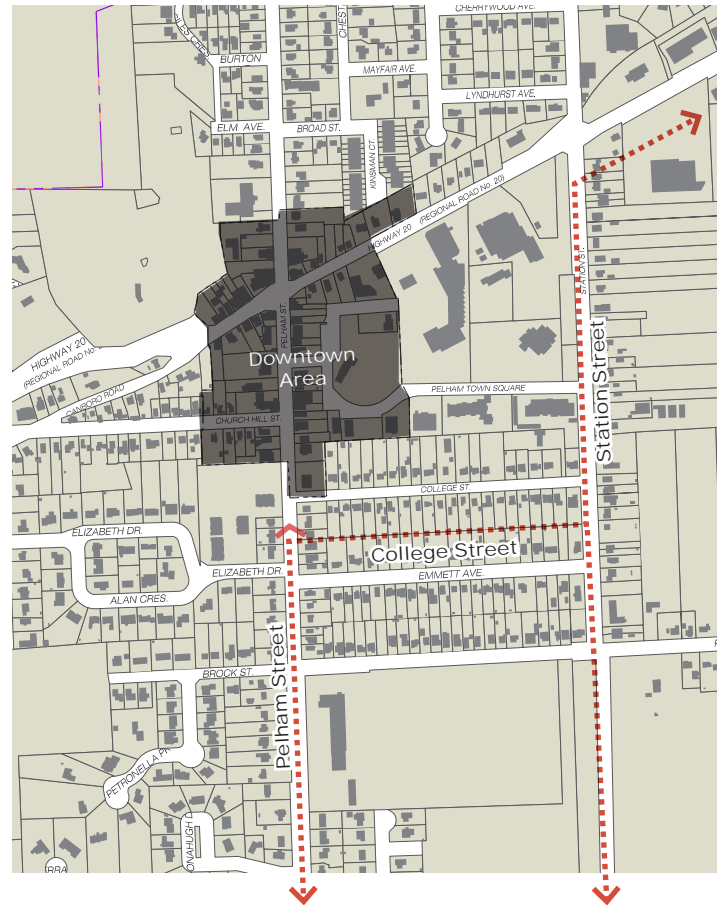


3.14 Bike Paths / Routes

The Steve Bauer Trail currently connects Pelham Street to Station Street via Pelham Town Square. There are no formal bike paths that clearly demarcate this route, either on- or off-street. Given the fast moving traffic, narrow sidewalks, the lack of a safe riding zone for cyclists and the importance of on-street parking for retail, it is recommended that bike paths be re-routed so that they lead to the downtown area, but not through it. To this end, Fonthill should be a destination for cyclists. As identified in the Cycling Route Plan, suggested cycling through-routes could use College Street to Station Street. East Fonthill also presents opportunities to enhance the network.

The following guidelines for new bike routes should be considered:

- Cycling paths that lead to the downtown, either on- or off-street, can be shared with pedestrian paths, but should be clearly identified either with a different pavement treatment or through signage.
- Accommodate highly visible, easily accessible and well-lit bike storage facilities at key locations such as Peace Park, along Pelham Street in Fonthill and Canboro Road in Fenwick, at Gateway locations, and at all key existing and proposed civic/institutional buildings.



Cycling Route Plan

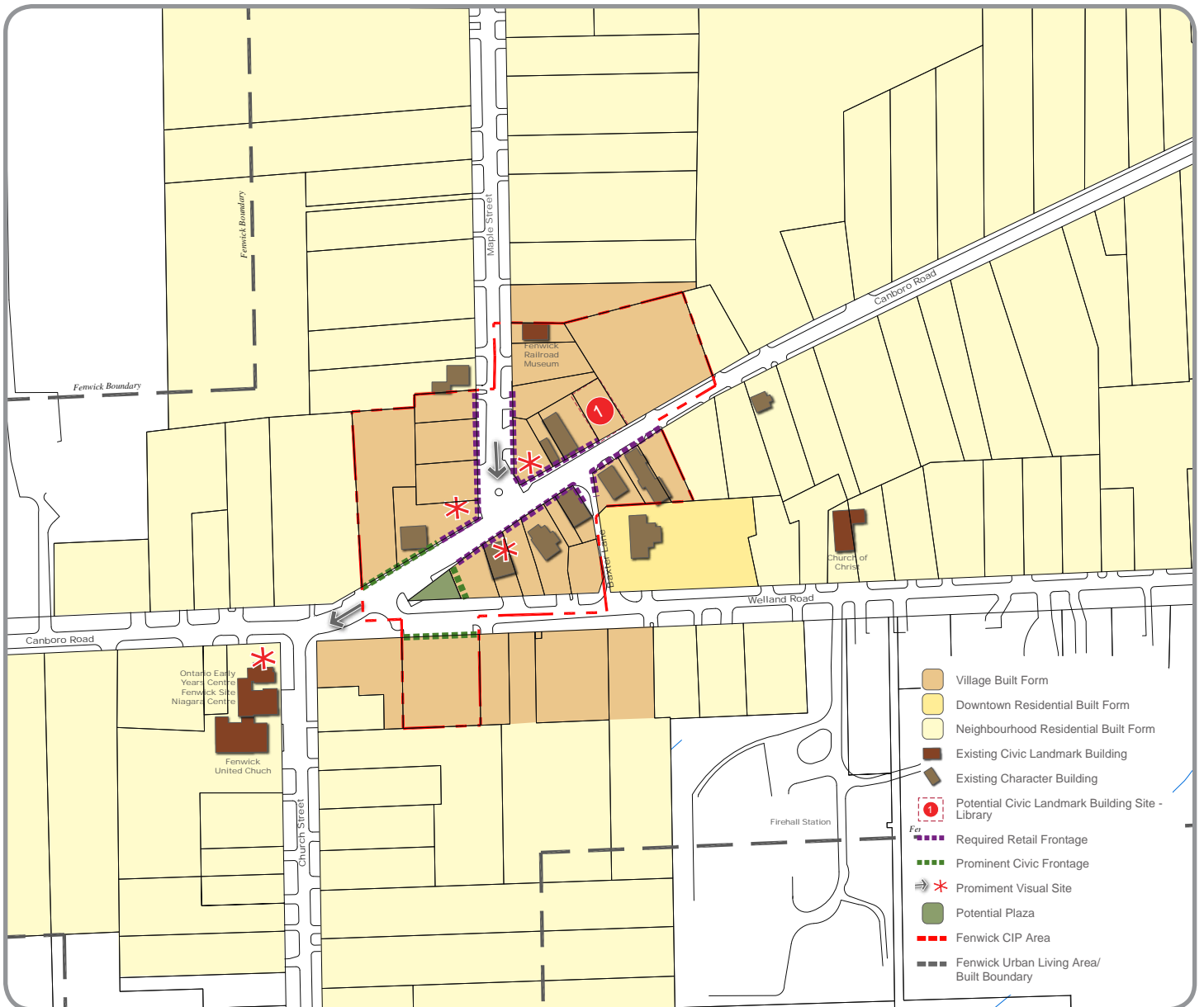


4.0 built form framework

4.1 Built Form Framework Plan

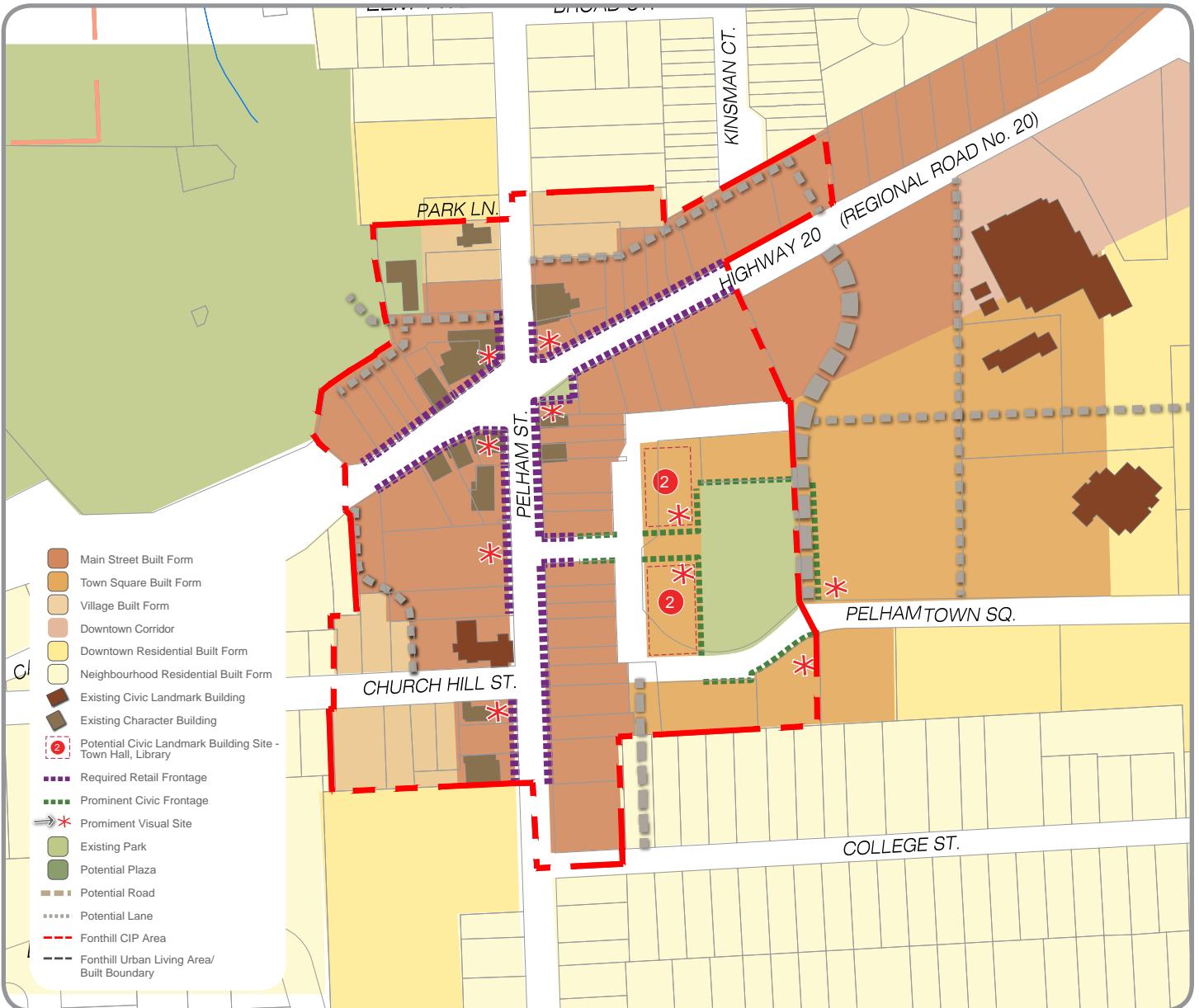
The Built Form Framework defines a structure for guiding future development in the downtown areas of Fenwick and Fonthill. The primary objective is to retain the defining areas built character, and ensure that any new development reinforces a coherent, integrated and appealing built

Fenwick Downtown Corridor Built Form Framework Plan



environment, as well as contribute to enhancing the public realm. The components of the Framework are described on the following pages. The Built Form Framework is also supported by the Downtown Design Guidelines (see Section 5.0 *General Downtown Design Guidelines* and Section 6.0 *East Fonthill Design Guidelines*).

Historic Downtown Fonthill
 Downtown Corridor Built Form Framework Plan





Continuous street wall and retail frontages create an inviting pedestrian environment in Niagara-on-the-Lake, Ontario



Provision for on-street parking reduces the number of driveways and minimizes the pedestrian-vehicle conflict in Perth, Ontario

4.2 Main Street Built Form

Main Street Built Form areas identify the lands in Fonthill that typify the traditional 'main street' configuration. These properties front onto both Pelham Street (just north of Highway 20 to College Street) and also along Highway 20 (between Marlene Stewart Streit Park and half a block east of Pelham Street).

These lands encompass the primary functional, symbolic and historic heart of the downtown area within the Town of Pelham. They include a mix of land uses, with retail most prominently at-grade with some office and/or residential uses above. The existing built form character varies greatly from one-storey commercial pads with parking in front; to traditional one- and two-storey street-oriented commercial buildings and house forms; to mixed-use buildings of up to three-storeys.

For the most part, these lots are characterized by 40 metre to 50 metre depths with frontages ranging from 8 to 20 metres. However in certain locations, particularly in the south-west quadrant of Pelham Street and Highway 20, the lot depths vary greatly from 35 metres up to 76 metres, and lot frontages reach up to 37 metres.

Where development is proposed within these areas, they should be subject to the following guidelines:

- New development should be a minimum of two-storeys and maximum of three-storeys. Modest exceptions in height up to four-storeys may occur at Prominent Visual Sites (see Section 4.12 *Prominent Visual Sites*).
- New buildings should be placed close to the street to a maximum setback of 1.0 metre.
- No interior yard setback is required.
- No parking should be permitted at the front of the building; all required parking should be handled through on-street parking or in consolidated rear parking lots.
- Access to surface parking lots should be accessed through rear lanes; no vehicular access or driveways should be allowed from the street.

4.3 Town Square Built Form

Town Square Built Form areas refer to the lands in Fonthill that surround Peace Park. To the south of the park, the area currently comprises of one-storey commercial and institutional buildings (library and post office); to the north and east, the park is fronted by a surface parking lot for a large one-storey commercial plaza, and a two-storey office building; to the west is the Town Hall and associated surface parking. The land uses and built form characteristics of these areas do not adequately address Peace Park as a celebrated public space in the heart of the Town Square (see Section 4.11 *Civic Frontage*).

As part of the strategy to promote civic presence and define prominent open space areas, new development proposed in these areas should be subject to the following guidelines:

- New development should be a minimum of three-storeys to a maximum of five-storeys in order to frame the public square; modest exceptions up to six-storeys may occur at Prominent Visual Sites (see Section 4.12 *Prominent Visual Sites*).
- New buildings should be setback from the front property line to a maximum of 2.0 metres.
- No parking should be permitted at the front of buildings. Required parking areas should be handled through on-street parking or in shared rear or side parking lots which should be appropriately screened with landscaping.
- Where possible, entrance to parking lots should be accessed through rear lanes to minimize the number of driveways.



Current built form conditions along Pelham Town Square:
One- to two-storey buildings set back far from the street, proliferation of front driveways and parking areas visible from the street



Examples of built form that addresses central public spaces and contributes positively to the public realm



Church Hill Street looking west, Village Built Form areas with residences with some converted into businesses



Examples of the retail streets that exhibit a residential character

4.4 Village Built Form

Village Built Form areas identify the lands in Fonthill that act as transition zones between the Main Street Built Form areas and the adjacent established residential areas. These properties front onto both main streets of Highway 20 and Pelham Street, but also onto Church Hill Street. The area is currently comprised of mainly one- to two-storey residences, with some homes converted into businesses. The defining feature is the “house” form: pitched roof lines; generous front yard setbacks; and, often a porch element.

As such, the Village Built Form area in Fonthill functions as a transition zone from the traditional main street (with smaller setbacks, more prominent street wall, and storefronts) into a built form that is more residential in character.

In Fenwick, Village Built Form areas account for most of the downtown. These properties are currently comprised of one- to two-storey commercial and office buildings, mixed-use buildings with retail at-grade, residences, and a variety of setbacks.

New proposed development in these areas should be subject to the following guidelines:

- New development should be a minimum of two-storeys and maximum of three-storeys.
- New buildings should consider architectural features which will positively contribute to the residential character of the area, including but not limited to: pitched roof lines, awnings, and front porches.
- New buildings should be setback more generously from the street between 1.0 metre to a maximum of 5.0 metres, which allows room for landscaped areas and/or patio spaces.
- Interior yard setbacks should be in the range of 0.6 metres to 1.2 metres.
- No parking should be permitted at the front of buildings; where possible, all required parking should be handled through on-street parking or in consolidated rear parking areas.
- Where possible, access to surface parking areas should be accessed through rear lanes in order to minimize the number of driveways.

4.5 Downtown Corridor Built Form

Downtown Corridor areas identify existing and proposed mixed-use lands outside of the historic Fonthill Downtown, comprised of the East Fonthill Secondary Plan area on the south side of Highway 20, as well as the properties along the north side, east of Station Street to Rice Road.

These lands hold great opportunity to expand and complement Downtown Fonthill by serving to provide significant new retail, office, residential, and institutional development and at a scale that could not be accommodated or is appropriate within the fine-grained smaller-scaled traditional downtown area. While the properties along the north side of Highway 20 are similar in scale and current uses to those west of Station Street within the Main Street Built Form area, there is the potential for redevelopment as East Fonthill is built out. The properties to the south side of Highway 20 are predominantly vacant and large contiguous land holdings that can be further subdivided by a grid of streets and blocks.

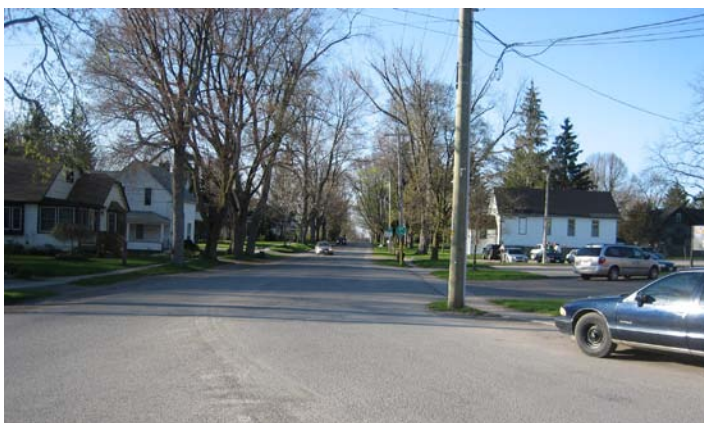
Where development is proposed within these areas, they should be subject to the following guidelines:

- New development should be a minimum of two-storeys and maximum of 6-storeys. Modest exceptions in height up to eight-storeys may occur at Prominent Visual Sites (see Section 4.12 Prominent Visual Sites).
- New buildings should be placed close to the street to a maximum setback of 3.0 metres.
- No interior yard setback is required.
- No parking should be permitted at the front of buildings; all required parking should be handled through on-street parking or in consolidated rear parking lots or interior courts for larger blocks.
- Access to surface parking lots should be accessed through side streets or rear lanes; vehicular access or driveways should be minimized on Highway 20 and should be shared where possible.
- Large format retail developments should be configured to enable further subdivision into smaller development blocks, with the Highway 20 frontage prioritized for future multi-storey mixed-use development (see Section 8.3 *East Fonthill Downtown Demonstration Plan*).





Canboro Road, well-preserved homes on tree-lined street



Maple Avenue, typical residential street in Fenwick



Downtown Residential Built Form, existing 3-storey and 4-storey low-rise apartment in Fonthill

4.6 Downtown and Neighbourhood Residential Built Form

In addition to the retail, commercial, and civic functions of Fenwick and Fonthill, there are pockets of built-up and stable residential areas located within and immediately adjacent to the study areas.

Downtown Residential Built Form areas are comprised of existing and proposed medium density housing forms. In Fonthill, they are comprised of three- to four-storey low-rise apartments and senior's housing, in addition to a townhouse complex located just south of Church Hill Street. The East Fonthill area also anticipates significant higher density development. In Fenwick, the area is comprised of the Maple Apartments located at Baxter Lane and Welland Road.

Neighbourhood Residential Built Form areas are comprised primarily of low density single-detached homes. It is in these areas where the rich and extraordinary history of Fenwick and Fonthill is most evident, as revealed in the remarkably well-preserved historic homes, landscaped gardens, and towering tree-lined streets.

The variety in housing forms afforded in both the Downtown and Neighbourhood Residential Built Form areas provides a range of housing choices to suit a diversity of needs and lifestyles. The proximity of higher densities support the local retail and commercial activities of the Downtown areas. Modest infill and redevelopment within existing Downtown Residential Built Form areas may occur where it can be accommodated. Ensuring the stability of these areas and the retention of their respective built form characteristics should be a fundamental objective when any alterations or developments are proposed.

4.7 Existing Civic Landmark Buildings

The presence of schools, places of worship and libraries introduce variety to the streetscape, provide amenity spaces, and lend to the diversity of uses and people in the Downtowns of Fenwick and Fonthill. These uses should continue to exist into the future and every effort possible should be made to physically enhance their landmark qualities through restoration, façade improvements and complementary landscaping. Where additions or external alterations are proposed, the design should be sympathetic and subordinate to the heritage aspects of the buildings while clearly distinguishing between that which is new and old.



Existing Fonthill Baptist Church

4.8 Potential Civic Landmark Buildings

There are several proposed development projects in both Downtowns that involve relocating public functions to new sites and/or combining civic roles into one new building. As highly visible public buildings, all new civic development should be designed with a clear identity, expressed through prominent architectural elements and high quality material. The following are potential civic landmark building sites identified in the Built Form Framework Plan:



Existing Maple Acre Library in Fenwick

Site 1 - Potential new Public Library/Community Function in Fenwick

As a new public building to replace the former Maple Acre Library, the proposed library site in Fenwick should be strategically located to maximize its visibility within the Downtown Area. As such, the potential new Library should be sited to reinforce the central focal point of Fenwick, directly across from the potential public plaza which makes it highly visible and easily accessible for residents. Special paving should extend from the plaza to the Library entrance, emphasizing the idea of connected civic uses. The building wall should be set close to the street, and the main entrance to the Library should face the street in direct view of the plaza. Streetscaping design should be consistent with the rest of the street, and should make further positive contributions to the public realm by providing more generous landscaping and street furniture such as benches and bike racks, in order to encourage public activity and use. Parking should be located at the rear of the building, concealed from view. To ensure the viability of this facility it should be combined with other community functions. The former library building should also continue to have a community oriented function.



Example of potential new Public Library



Existing Town Hall in Fonthill



Existing Public Library in Fonthill



Examples of new consolidated Town Hall and Library building in Fonthill

Site 2 - Potential new consolidated Town Hall & Library Building in Fonthill

The Built Form Framework Plan identifies a new Town Hall and Library building on the site where the current Town Hall is situated. Rather than facing the rear of buildings as it currently does, however, the new building will front onto Peace Park and frame the open space. Active uses, like a small café, should be encouraged at grade-level in order to provide year-round pedestrian activity. The building will be placed close to the property line, with ample windows overlooking and multiple pedestrian entrances from the Public Square and Peace Park. Pedestrian mid-block connections should lead to the Town Square, and connect pedestrian movement from the streets to the Town Square. For vehicles, the building will be accessed by shared a rear lane to underground public parking. With its direct linkage to Peace Park, the new consolidated Town Hall / Library building should strengthen the notion of a prominent and accessible civic node for Pelham.

Site 3 – New Community Centre and Arena

A key community facility is proposed for the East Fonthill area that will include a variety of functions such as ice pads and community meeting spaces. Strategically located as an east anchor to the 'Civic Corridor' that links to Pelham Town Square, as well as on a proposed new Public Square, this facility should be carefully designed to respond to its highly visible location. At a minimum, it should orient to and activate the adjacent streets and the Public Square, and its architectural design should reinforce its civic importance as a community focus for this part of the Greater Downtown Fonthill area.

4.9 Character Buildings

Heritage is most often cited as the catalyst for many successful downtowns for small towns across North America. As demonstrated in Ridgeville, the charm brought about by heritage sensitive improvements and renovations to façades can provide for great appeal and become an attraction. Although no listed or designated buildings have been formally identified in the Downtown Areas of Fenwick and Fonthill, historic photographs reveal that a significant number of original structures of the settlement still exist.

The Built Form Framework Plan identifies a number of Character Buildings, that with improved façades, have the potential to positively contribute to the streetscape or the history and identity of downtown Fenwick and Fonthill. While Character Buildings traditionally have architectural attributes that include high quality materials, distinctive rooflines, and/or highly articulated features such as cornices, porticos, and windows and traditional shop fronts, few buildings in the Downtown Areas have retained their architectural integrity in the façades. Over time, buildings have been rebuilt and resurfaced, and many covered up with paint, vinyl siding, corrugated steel or otherwise.

However, it should be recognized that a number of buildings and/or sites hold historical significance, and that some buildings may still retain their original structure. As a result, when any redevelopment or where adjacent development is proposed, an assessment should be made to determine what, if any, aspects of these buildings should be protected. Where heritage significance is confirmed, it is recommended that these aspects be designated. (See Section 5.11 *Restoring Existing Buildings*).



Preserved historic buildings in Ridgeville, Canboro Road



Recently renovated buildings with façade improvements in Fonthill.



Example of narrow, consistent retail frontage along a main street, City of Welland



Retail frontages along Canboro Road, Fenwick

4.10 Retail Frontage

In spite of the current 'gaps' in the street wall created by vacant lots, blank walls, driveway access, and front parking areas of both Fenwick and Fonthill, the ultimate goal is to make the downtown areas a successful pedestrian environment and a vibrant shopping destination to draw in local residents and visitors alike. In order to achieve this objective, it is crucial to further promote and enhance the continuous rhythm of small-scale shops and restaurants as a defining characteristic of the downtowns.

The Built Form Framework Plan identifies required retail frontages along properties that front onto traditional retail streets, namely Pelham Street and Highway 20 in Fonthill, and along Maple Street and portions of Canboro Road in Fenwick. At-grade retail uses that address the sidewalk should be a requirement for these frontages in order to ensure a continuous, active streetscape. The character and design of the storefronts in these locations is also important to the atmosphere and character retention of the streetscape (See Section 5.4 *Storefronts*).

A new potential main street character area is also proposed for East Fonthill, centred on a new public square (subject to guidelines in Section 6.0).

General guidelines for these Retail Frontages include:

- Maximum 2,500 sq.m retail space for Main Street Built Form along Highway 20 in Fonthill.
- Maximum 500 sq.m retail space for Main Street Built Form along Pelham Street in Fonthill, and Village Built Forms in both Fenwick and Fonthill.
- Articulation of narrow storefronts consistent with current conditions in Fonthill, generally in the range of 8 to 12 metre frontages. In Fenwick, wider frontages ranging from 8 to 14 metres.
- Second storey and/or double storey commercial uses are encouraged to intensify the retail presence and activity while providing for alternative and affordable retail space.
- Spill-out activity such as sidewalk cafés are highly encouraged where they can be accommodated.
- Weather protection for pedestrians is encouraged through the use of awnings and canopies.
- Commercial signage should be of high quality and should add diversity and interest to the streetscape.

4.11 Civic Frontage

Civic frontages are comprised of land which surround important civic open spaces. In Fonthill, these are mainly properties that frame Peace Park. In Fenwick, these are properties that front onto the proposed plaza at the junction of Welland Road and Canboro Road. In both cases, new proposed development will be highly visible and will have a direct impact on the quality of public space that they address. Guidelines for Civic Frontages include:

- Frontages should directly relate to the open space, with articulated front entrances and ample windows.
- Large parking areas and driveway access visible from the park/plaza should be discouraged.
- Continuity of architectural features such as arcades.
- Highest quality in design and materials.
- Uses spill out actively to animate the space in all seasons and hours.





Prominent visual sites which terminate a view, like the Town Hall site in Fonhill, should be designed with high quality materials and architectural treatments



Terminus views are highly visible, and are ideal locations to locate special architectural treatments or gateway features, like a clock tower.

4.12 Prominent Visual Sites

Highly visible building sites at gateways, key corners, on public spaces and at the terminus of streets are identified for special architectural treatments to enhance the quality of the public areas, strengthen the Downtown's distinct identity and serve as orienting devices for pedestrians. These sites should permit distinct massing responses in addition to encouraging high quality material and architectural design. Accordingly, an additional storey should be permitted subject to the General Downtown Design Guidelines (See Section 5.3 *Height and Massing*).



5.0 general downtown design guidelines historic fenwick and fonthill

The General Downtown Design Guidelines Historic Fenwick and Fonthill provide appropriate standards or benchmarks for new development primarily within the Historic Downtown Areas of Fenwick and Fonthill. They are informed by best practices in urban design but tailored where necessary to the unique conditions of Fenwick and Fonthill.

These guidelines help to guide and shape new buildings in order to reinforce the objectives of the Built Form Framework and environmental sustainability. At the same time, they provide flexibility within certain parameters to encourage distinction, variety, in addition to creative architectural responses. Where developments do not conform to the guidelines but propose alternative standards, they should be assessed to ensure that the intent and spirit of the guidelines are met with respect to their primary objectives.

5.1 Building Elements

To encourage continuity in the streetscape and to ensure horizontal 'breaks' in the façade, buildings should be designed to reinforce the following key elements through the use of setbacks, extrusions, textures and materials:

top

The roof condition, expressed as an upper storey or roof feature, should be distinguished from the rest of the building and designed to contribute to the visual quality of the streetscape.

middle

The middle or body of the building should contribute to the physical and visual quality of the overall streetscape.

base

A base should be clearly defined that positively contributes to the quality of the pedestrian environment in the level of animation, transparency, articulation and material quality.



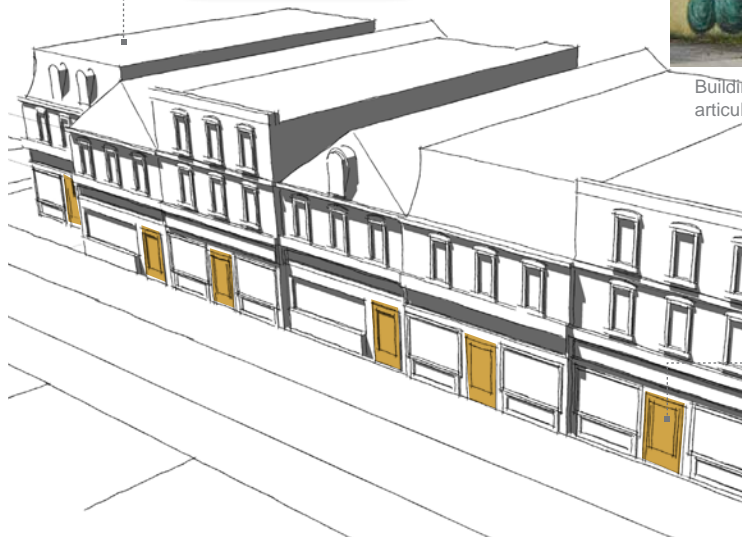
5.2 Orientation & Placement

The orientation and placement of buildings along the street help to reinforce the public realm by enhancing the pedestrian environment through creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings, providing the appropriate levels of animation and use. Key guidelines for the orientation and placement of buildings are as follows:

- All buildings should orient to and address the street with clearly defined entry points that directly access the sidewalk.
- Buildings should be placed at or close to the street edge, consistent with adjacent buildings.
- Development of an entire block or at corner sites may provide greater setbacks to widen sidewalks without compromising the visual continuity of the streetscape.
- To discourage fragmentation of the street wall and to encourage full utility of the rear lanes, 100% building coverage of the front-yard is encouraged, especially for those with retail frontages.

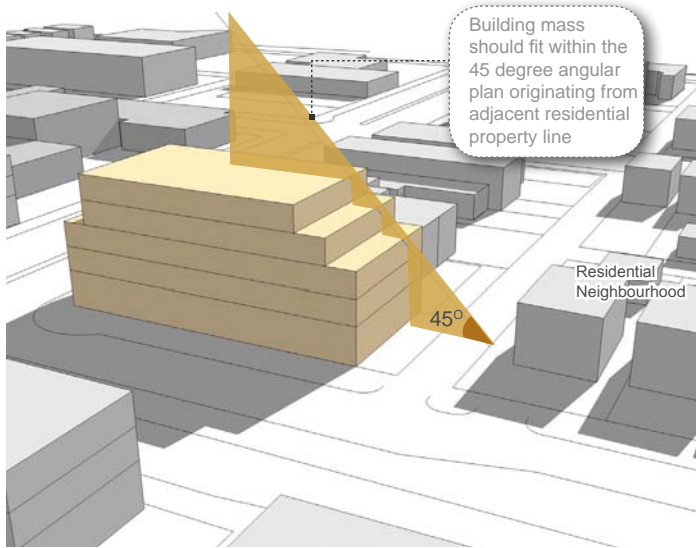


Corner buildings can setback further from the street in order provide potential spill-out uses like patios and cafés



Buildings are placed close to the street edge and address the street with clearly articulated entrances

Front entrances to retail stores should be clearly defined and directly accessible from the street



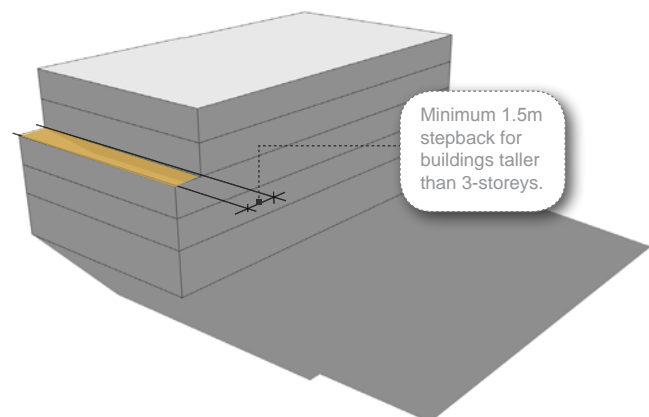
5.3 Height & Massing

Buildings heights play an important role in the impact on the character and quality of the street experience. The height and mass of the building can ensure visual continuity, maintain pedestrian scale, animation of the street and design quality. Guidelines for building massing include:

- Buildings taller than three-storeys should provide a minimum 2.0 metre stepback at the front building wall at the second or third storey.
- 45-degree angular planes should be applied where properties abut neighbourhoods.

Where new development is proposed, guidelines for height is summarized in the chart below, organized by the built form categories as identified in the Built Form Framework.

Built Form	Minimum Height (No. of Storeys)	Maximum Height (No. of Storeys)	Prominent Visual Sites (Max No. of Storeys)
Main Street Built Form	2	3	4
Town Square Built Form	3	5	6
Village Built Form	2	3	4
Downtown Residential Built Form	3	4	
Neighbourhood Residential Built Form	1	3	





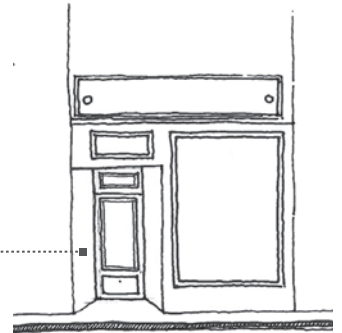
Signage should add diversity and interest to the street, but not overwhelm the storefront

Awnings for weather protection

Break between street-level uses and upper floors through change in material to emphasize store front and add variety to streetscape

High level of window transparency (minimum 75%) for maximum visual animation

Highly visible storefront located near or at-grade level



5.4 Storefronts

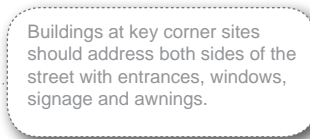
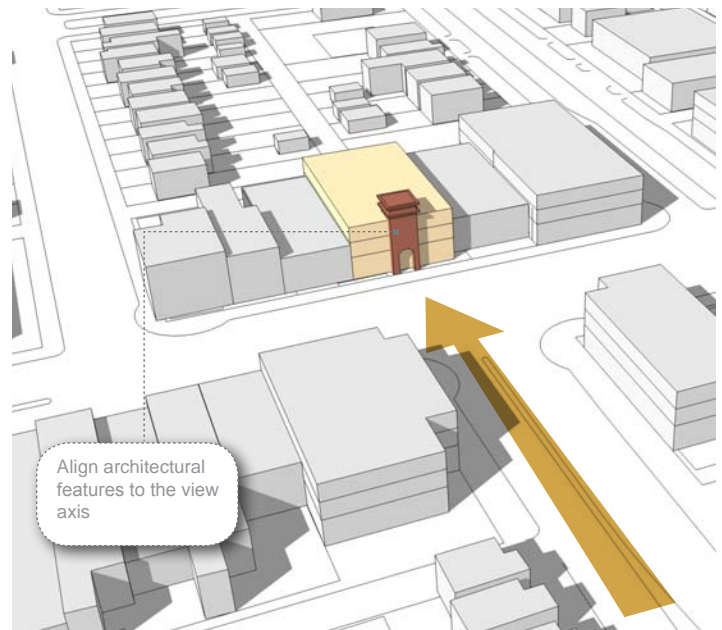
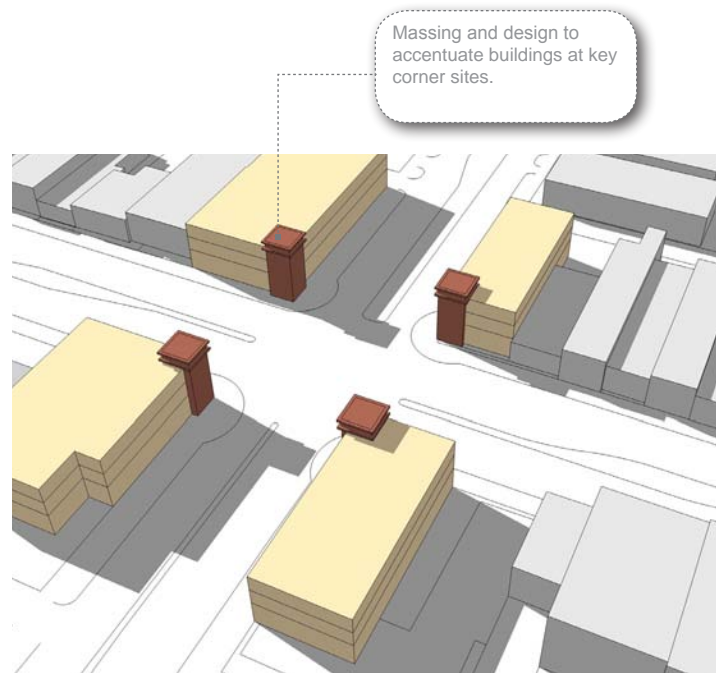
Well-proportioned and designed storefronts can contribute positively to the pedestrian environment by providing animation and visual interest at the sidewalk. A defining characteristic of a traditional main street is the mix and variety of storefront styles and types, in addition to the narrow widths and high level of transparency that lends to the vibrancy and animation of the street. New development should reinforce these characteristics according to the following guidelines.

- To reflect the existing character and context, storefronts should generally have a frontage in the range of 8.0 metres but not greater than 14.0 metres.
- Weather protection for pedestrians is encouraged through the use of awnings and canopies.
- Where retail frontages are greater than 8.0 metres, they should articulate narrow storefronts in the design of the façade.
- To ensure a strong and attractive visual presence on the street and a high quality retail space at grade level, a minimum 4.5 metre floor to ceiling height should be provided.
- Storefronts should have a high-level of transparency, with a minimum of 75% open glazing to the interior of the building to maximize visual animation.
- Clear glass should be used for wall openings (e.g., windows and doors) along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts. Limited artistic etching permitted.
- An identifiable break or gap should be provided between the street-level uses and the upper floors of a building. This break or gap may consist of a change in material, change in fenestration, or with the addition of a cornice line. The identifiable gap or break can emphasize the storefront while adding visual interest and variety to the streetscape.
- Continuous awnings and canopies should provide an identifiable break or gap between storefronts.
- Awnings should be no more than 25% of the front façade.
- Awnings should be made of high quality canvas, cloth, metal or glass material, and contain a maximum of two colours.
- Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade. Split level, raised or sunken entrances are strongly discouraged.
- On corner sites, storefronts should address both street frontages through entries or glazing.
- Storefront signage should be consistent with the signage guidelines (See Section 5.8 *Signage*), but generally should add diversity and interest to the street and not overwhelm either the storefront or the streetscape.

5.5 Prominent Visual Site Treatment

Sites located at key corners and at the terminus of view corridors have greater visual prominence. While corner sites have frontages onto two streets and frame intersections, view terminus sites can be highly visible from great distances. Those sites can help to enhance visual connectivity and orientation. Accordingly, they have a greater civic obligation and should be designed to give good form and prominence to the sites they occupy.

- To enhance the distinction of new buildings at Prominent Visual Sites, modest exceptions to setbacks and height restrictions should be permitted to encourage massing and designs that accentuate the visual prominence of the site – architectural treatments can include tall slender elements such as spires and turrets.
- New development on terminus sites should align design features to the view axis which, in addition to tall architectural elements, can include aligned entries or portico openings.
- New developments on all corner sites should orient to both street frontages.
- Corner entrances should be encouraged wherever possible, to give address to both street frontages.
- As new developments on prominent visual sites can shape the image and character of an area, the highest possible standards in design and material quality should be encouraged.
- Architectural lighting should be encouraged on prominent visual sites.





5.6 Roof Lines

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs of buildings are also seen from other buildings of equal or greater height. Roof design should consider the following guidelines:

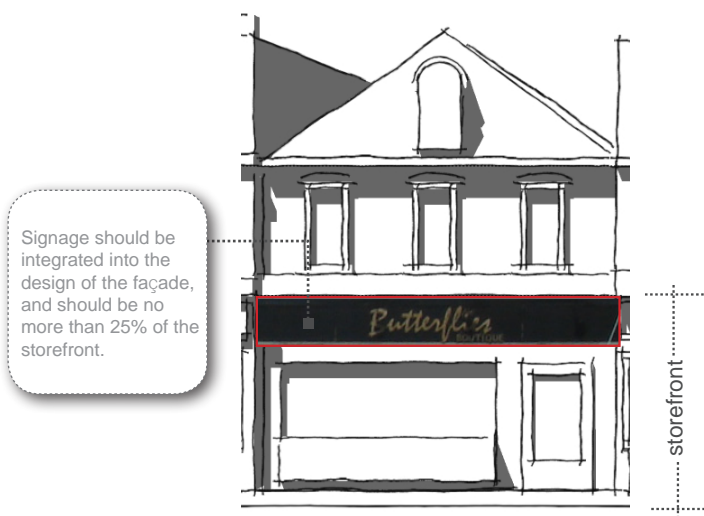
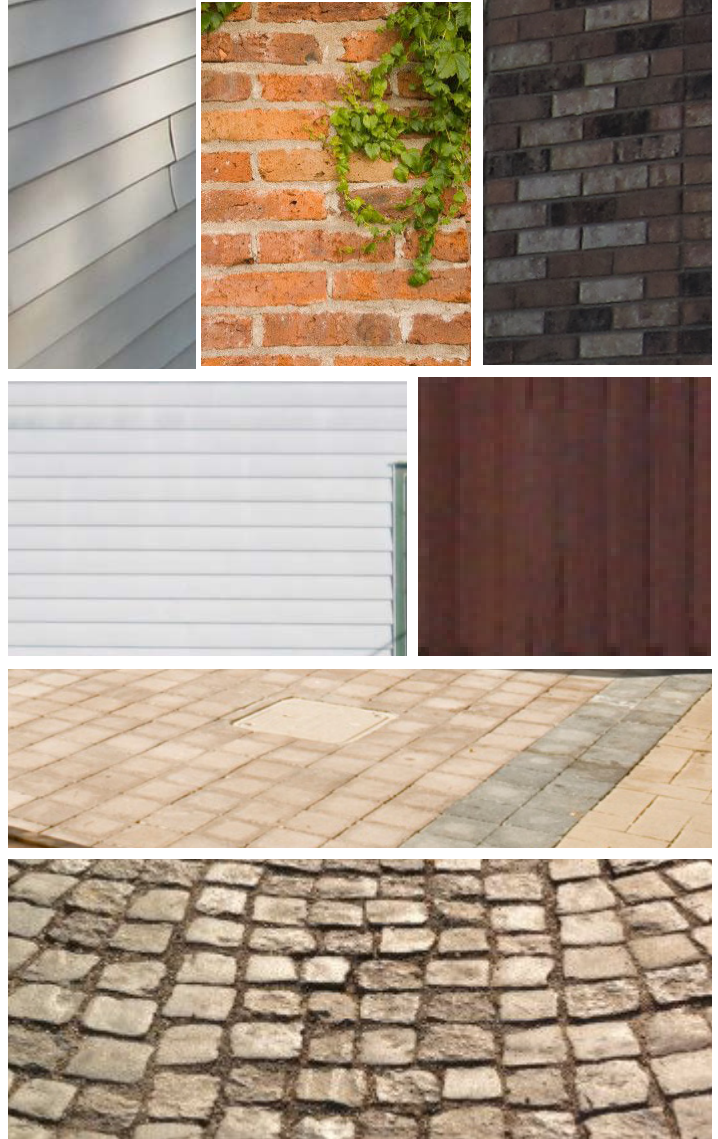
- The expression of the building top and roof should be clearly distinguished from the rest of the building through treatments such as stepbacks, change in materials, cornices lines, and overhangs.
- Rooftop mechanical should be integrated with the architectural treatment of roofs and/or screened from view for all sides.
- Green roofs should be encouraged to provide for amenity and to meet sustainable objectives.



5.7 Material Quality & Colour

New development should be mindful of ensuring excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Design Guidelines for Fenwick and Fonthill is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- Fenwick and Fonthill have a rich history of architectural styles, colours, and material quality from which to draw from. New developments should seek to contribute to this mix and variety.
- Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete.
- In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- Vinyl siding, plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.
- Use of stucco should never exceed 50%, in order to maintain existing community texture and feel.





High quality and well-maintained projecting/hanging signs maximizes the visibility of the retail street, and are sometimes secondary to a wall sign.



Examples of wall mounted signs. While wall signs are mounted flush and fixed securely to a building wall facing the street, an awning sign is typically printed or painted on, or attached to an awning or canopy above a business door or window, and are predominantly oriented to pedestrians from the opposite side of the street.



Moveable signs like sandwich boards stand on their own typically below eye level, are not substitutes for permanent identification signs.

5.8 Signage

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape, and should reflect the unique characteristic of their context. This includes compatibility with existing buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged. Commercial storefront signage should be subject to the following guidelines:

General Guidelines

- Commercial signage should not overwhelm the building and/or the storefront.
- Back-lit illuminated rectangular sign boxes are discouraged.
- Signs should be well-maintained and constructed using high quality materials.
- Signage should aid pedestrians and drivers in navigating the area, especially at night.
- Commercial signage should be no more than 20-25in tall on a building façade frontage or exterior side yard.

Sign Placement

- Signs should be placed in a consistent location on all building façades. Generally, it should be located above the storefront windows or on canopies over the storefront. The Town should define a sign band area.
- Signage should not obscure windows, cornices or other architectural elements.
- To minimize visual clutter, signage should be integrated into the design of building façades wherever possible, through placement within architectural bays and friezes.
- The maximum signage area for storefront signs should be no more than 25% of the business storefront at the ground-level.
- Signage on character buildings should be consistent with traditional sign placement such as on a sign band, window lettering, or within the existing architectural orders.

Sign Types

- Large freestanding signs (such as pylons), roof signs, and large-scale advertising (such as billboards) should be discouraged.
- Projecting/hanging signs should be permitted to encroach over the streetline provided that they do not project more than 1.0 metre from the building, and they have a minimum 2.4 metres clearance between the bottom of the sign and grade.
- Moveable signs like sandwich boards should have two sign faces, and be located in front of the associated business on private property where possible, but are permitted to encroach onto public property provided that they provide a minimum 1.5 metres of unobstructed sidewalk space.

5.9 Access, Parking & Service Areas

Access refers to the location and scale of the pedestrian and vehicular entry to a lot and/or building. The primary objective with respect to parking and servicing in the Downtowns is to minimize their visibility and potential conflicts with pedestrians. New development should follow these guidelines:

- Where possible, parking for new development should be provided below-grade or to the rear and accessed by a rear lane.
- Where they currently do not exist, new development should introduce rear lanes whenever possible; otherwise, an adequate easement (minimum 7.5 metres) in the appropriate location should be maintained to protect for a future potential alignment.
- Surface parking and service areas should be located to the rear of a development and generally not visible from the street.
- Especially for narrow lots, individual business / land owners are encouraged to consolidate parking areas between contiguous lots. At a minimum, adjoining sites should share driveways into parking areas.
- Where surface parking or service areas are exposed, they should be concealed with landscaping and/or other mitigating design measures.
- Direct access to parking or service areas from streets should be discouraged and not permitted for lot frontages that are less than 30.0 metres in width.
- Pedestrian building entrances should be primarily from the street, and automobile access primarily from secondary local streets or rear lanes. Additional building entrances may be oriented toward the rear or side of the building for access to and from parking lots.
- Clear pedestrian routes should be provided from parking areas to main entrances.
- Pedestrian walkways between buildings should provide for a clear view, from one end to the other, to ensure pedestrian safety.
- Loading areas should be screened from adjacent properties and public view through the use of fences and landscaping.
- Loading and drop-off access should be from rear lanes or secondary roads.
- Garbage areas should be fully enclosed and attached to the primary building.



Consolidated parking areas located at the rear of the properties, accessed by a rear lane.





A potential site for infill



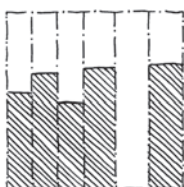
Consistent building and ground storey heights



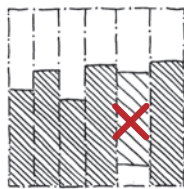
Consistent horizontal alignment of façade elements



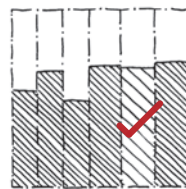
Consistent storefront and window placement with appropriately proportioned window openings



A potential site for infill



Improperly sited development, stepped back from the street line



Properly sited development, built to the street line

5.10 Infill in Existing Heritage Contexts

The Built Form Framework Plan identifies a number of Character Buildings that make positive contributions to the history and identity of the Downtown Areas. Where a new building is proposed, especially for those adjacent to a Character Building, its design should be sensitive and complementary. The following guidelines help to ensure the fit for new buildings so that they contribute, rather than detract, from the distinct character of historically significant properties.

General guidelines

- New buildings should avoid historical misrepresentation. Buildings tell the story of historical development of the area. It is important that the historical record does not get confused through the mimicry of past architectural styles.
- New buildings should be designed so that they do not appear to have been constructed earlier than they were.
- New buildings should consider and respect the scale, material and massing of adjacent buildings.
- New buildings should be located at a distance from the curb equal to that of adjacent buildings.

Façade Articulation

- New buildings should respect the pattern of façade division by ensuring the horizontal and vertical architectural orders are aligned with neighbouring buildings.
- New buildings should have entries and display windows at regular intervals consistent with the established pattern on the block.
- Windows should be vertically aligned from floor-to-floor and horizontally aligned with the neighbouring buildings.
- New buildings should include a cornice that is carefully aligned with neighbouring buildings and of similar proportions.
- Change of plane to create articulation on the façade is encouraged.

Façade Materials

- New buildings should consider the palette of materials and colours evident in existing buildings.
- Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, sustainability and ease of maintenance. Materials should also be chosen for permanence. Vinyl siding, plaster, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.

5.11 Restoring Existing Buildings

The financial incentive programs recommended in the Town of Pelham Community Incentives Program allow business owners to improve the condition, physical appearance, and architectural character of commercial and mixed-use buildings and properties, including building façades. As a result, for properties where redevelopment will not likely happen in the short-term, property owners can benefit considerably from the loans and grants made available from the Community Incentives Program to make improvements to the exterior of existing buildings. Most immediately, these improvements could occur in the form of façade improvements including enhancements to commercial signage and storefronts and/or displays.

Where building restoration projects are proposed, they should contribute to the character of the street, and follow the Downtown Design Guidelines identified in this report.

Below is an illustration of how the principles and general guidelines could be applied to an existing building in Fonthill.

Application of the Guidelines to an Existing Building



1. Existing Condition

The building on the left does not relate well to its context, creating an inconsistent retail façade from the building on the right. Both upper level and lower level windows are out of scale.



2. General applied guidelines

- Differentiate the base of the building from the upper levels.
- Generate a consistent horizontal pattern by aligning sign bands, storefront windows, and the base element.
- Front entrances should be a primary design consideration, not an afterthought. Recess entries to reinforce their importance on the street and to generate a true sense of entry.
- Enlarge storefront windows at street level to maximize animated displays and transparency.
- Align windows to adjacent buildings to ensure consistency.
- Provide weather protection for pedestrians, such as awnings.



3. The Outcome

The building on the left is now, from an urban design perspective, consistent with the one on the right. Most prominently, the storefront elements including windows, door entry, as well as signage location and design are continuous with the correct proportions and alignment.

5.12 Environmental Sustainability

Sustainable development is development that meets the needs of the present without compromising the ability of future generations to meet their needs. As identified in the *Regional Municipality of Niagara Model Urban Design Guidelines (2005)*, environmental sustainability is a key design principle, and has important urban design implications.

For the most part, the Downtown Areas of Fenwick and Fonthill have already achieved one crucial sustainability principle, that being the intensification of existing urban areas to conserve land. By focusing on improving and intensifying the existing Downtown Areas, existing infrastructure and resources are better utilized, while reducing the need for urban expansion and its pressure on natural features, as well as rural and agricultural lands.

Furthermore, the co-location of complementary uses to reduce the need to travel long distances are currently being achieved, albeit minimal. As this Downtown Master Plan maintains, greater intensity and variety of uses should be realized, and better pedestrian and vehicular connections made.

Sustainable design can be applied to both the private and public realm, and encompasses streets, parks, and buildings. As part of the strategy to achieve environmental sustainability in both public and private development, new development proposed should be subject to the following guidelines:

- Paved areas (such as parking lots, driveways, and streets) should be minimized wherever possible and should be constructed of permeable materials and porous surfaces in order to reduce the volume of runoff into the storm drainage system.
- Wherever possible, vegetative or grassy swale drains should be considered and incorporated into the road design for non-urban road cross sections and in surface parking areas in order to maximize water infiltration and cleansing runoff.
- Where feasible, new street trees and landscaping should be of native plant materials and grown naturally within the Region.
- Light coloured materials should be used for parking lots; the use of blacktop should be avoided.
- Streetscaping and building elements should be chosen for their whole life cycle costing, and determined by suitability, durability, ease of maintenance, and cost effectiveness.

- Native, local, indigenous, and post-consumer recycled building materials should be used wherever possible.
- Bicycle storage facilities should be provided at public parks and public buildings in order to encourage alternative modes of transport.
- Wherever possible, new development should seek LEED certification, ensuring higher performance standards in environmental responsibility and energy consumption.
- Green roofs should be considered to minimize water runoff and improve building insulation.
- Adaptive reuse of existing buildings (all, or partial) should be considered not to only minimize demolition and reduce dependence on new materials, but also help to preserve the historic features of the structure and neighbourhood.
- Wherever possible, buildings should be designed with large south-facing glazed surfaces in order to maximize solar orientation.



Example of vegetative or grassy swale drain



Example of adaptive reuse to preserve historic structures and neighbourhood

5.13 Standards for Crime Prevention Through Environmental Design (CPTED)

CPTED is a multi-disciplinary approach to deterring criminal behavior that relies upon the environment to influence a potential offender's choices, prior to committing a criminal act. It is based on the notion that instead of a reactionary disjointed response to crime, that typically results in increased fortification and fear of crime, a set of scientifically proven principles can be employed to better develop a site, which will positively influence the likelihood of proper and appropriate behavior on that site.

CPTED has been proven to work, and to this end, the application of these principles on all new developments and/or re-developments will over time, reduce the propensity of crime and fear of crime throughout the Town.

Principles

1. Natural Surveillance

Maximizing visibility and the opportunity for observation through the placement and design of physical and social features. This includes the placement of gathering spaces/points of interest, building orientation, lighting, windows, entrances/exits, parking lots, walkways, security stations, fencing, landscaping, vegetation, signage, as well as any other physical obstructions. This principle helps create the perception of risk to an offender.

2. Natural Access Control

A logical and organized design to restrict, encourage, and safely channel the movement of people and vehicles into, out of, and within a site in a controlled manner. Natural Access Control should be coupled with Natural Surveillance techniques. This principle helps create the perception of control over the offender, as well as more easily identify those who venture into areas they should not be in.

3. Territorial Reinforcement

Defined property lines and clear distinctions between public, semi-private, and private spaces through physical or visual design can create a sphere of territorial influence that can be perceived by, and may deter potential offenders. Territorial reinforcement can be created using among other things, landscaping, pavement designs, gateway treatments, signs and fences. This principle helps create a sense of

proprietorship by the rightful user and helps more easily identify those who venture into areas they should not be in.

4. Maintenance

Well-maintained buildings and grounds inform potential offenders that 'Someone is Home'. It also reinforces the space's intended use and purpose. This principle helps create a sense of occupancy to the offender, as well as guardianship and proprietorship by the rightful user.

5. Logical Fortification (Quasi-Principle)

Fortifying buildings or property by the use of materials and/or equipment. The logical use of fortification should never conflict with any other CPTED principle. This quasi-principle although not widely held as a 'base principle' of CPTED is nonetheless important to deter or even prosecute criminal activities. This quasi-principle helps create a sense of obstacle or inconvenience to the offender.

General Provisions

- The Town of Pelham Crime Prevention Through Environmental Design (TOP-CPTED) standards shall apply to all non-residential and multi-residential developments and/or redevelopments subject to Site Plan Control, however respect for existing development and existing conditions will be considered.
- All applicable developments shall employ TOP-CPTED standards in a coordinated fashion over the entire site.
- It is strongly encouraged that all Site Plan applications be designed with the advice of a qualified CPTED trained professional to minimize possible drawing revisions due to TOP-CPTED issues.
- In instances where the TOP-CPTED standards conflict with other Department comments or Town policies, standards, and/or requirements, the Director of Community Planning & Development will determine which policy/standard/regulation is paramount.

TOP-CPTED Guidelines

- All sites must employ and enhance both natural and designed surveillance opportunities. These opportunities should, by design, be focused on places of criminal interest in addition to entry/exit points.
- Commercial buildings should provide maximum window coverage on all front and side first storey façades (facing higher traffic areas) to promote surveillance of both interior and exterior views of the building(s). As a general rule, glazing should allow an interior visibility of a building to a minimum 6' (1.8 metre) depth.
- Sites should be designed to limit visual barriers of sightlines from the street to building exit/entry points.
- Walkways/pathways to and from buildings should be located in areas supported by lighting and surveillance, but should not be located near ambush points.
- All site lighting must adhere to the Site Lighting Standards of the Town of Pelham, which include design requirements for:
 - Fixture (luminaire);
 - Mounting;
 - Light Source (Lamp);
 - Illumination Levels; and,
 - Photometric Plan Submission Requirements.
- Points of building access should be located in areas of high visibility and the site should be designed to enhance attention to these areas.
- Points of access to a building should be limited in number and centralized where possible.
- Sites that utilize or design private or semi-private space that abuts public space shall employ territorial markers.
- Sites that utilize or design private space that abuts semi-private space shall employ territorial markers.
- Bicycle storage shall be located in areas of illumination that have high visibility.
- All fencing, other than privacy fencing, shall

employ approximately 20% openings portioned throughout.

- Sites should be designed to facilitate easy maintenance.
- Use of defensive (thorny) vegetation is permitted in areas requiring defense measures, such as along interior fence lines, under accessible windows, or bordering outdoor storage areas, and may also be permitted in limited supply as an interior barrier to private spaces.
- Decorative ground or non-building façade lighting should be placed behind the feature (away from the street) to illuminate and reduce any potential hiding or stealth movement areas.



Example of bicycle storage rack



Example of promotion alternative modes of transport

5.14 Standards for Healthy Communities

Healthy communities provide opportunities for walking and exercise in a safe and comfortable environment. The guidelines provided in this Section are intended to improve pedestrian and cyclist comfort and enjoyment, and are intended to work in conjunction with the other guidelines provided in this document.

Street Connectivity Standards

- Infill development should identify opportunities to increase street connectivity.
- Street networks and off-road paths in greenfield development should always:
 - Provide the maximum choice for how people will make trips;
 - Take full account of the kinds of movement a development will generate; and
 - Make clear connections to existing routes and facilities.
- Cul-de-sacs are not permitted unless required for technical reasons.
- Crescent streets, reverse frontage lots and loop roads must not constitute more than 20% of total street frontage and should be discouraged.
- Blocks in the proposed development must not exceed 80 m x 150 m in size. Exceptions are made for blocks consisting solely of Parkland or of Employment uses.
- Intersections should be frequent, with street blocks decreasing in size as density increases.
- Sidewalks, bike lanes and multi-use paths should connect to street networks, major destinations and transport nodes.

Sidewalk Amenities

- All streets must have sidewalks on each side that are at least 1.5 metres wide in low-density residential areas, and at least 2 metres wide in medium-density residential neighbourhoods, high density residential neighbourhoods, mixed use areas and commercial areas.
- A variety of street trees that are hardy, resilient and low maintenance should be planted at regular

intervals (as specified by the municipality) adjacent to all streets.

- Transit shelters and other street furniture should be provided, especially on major pedestrian routes. Other street furniture may include benches, waste receptacles, newspaper outlets, community information boards, water fountains, public washrooms, bicycle parking, and bicycle sharing system components.

Cycling Amenities

- A connected and destination-oriented bikeway network should be provided throughout the community, including a variety of on- and off-street bikeway facilities that provide an appropriate degree of separation from motorized traffic, given the speed and volume of traffic on the street. These on-street bikeway facilities may include (but are not limited to) bicycle lanes, cycle tracks, sharrows, signed routes, bicycle boulevards and multi-use paths on the boulevard.

Where there is a local Bicycle Plan, the bikeway network proposed in the Plan should be implemented in the development area, and opportunities to enhance or connect to the proposed bikeway should be identified.

- At a minimum 100% of the population should be within 150 metres of a continuous and connected bikeway facility.

Intersections

- All intersections should be designed to increase the visibility of cyclists and pedestrians, give them priority, reduce crossing distance and provide adequate crossing time. Intersection design elements may include, but are not limited to:
 - Pavement treatments and markings for pedestrian crossings (e.g. brick paving, zebra/ladder markings)
 - Curb cuts/ramps
 - Raised crosswalk

- Curb extension/bulb out
- Centre Median or refuge island
- Pedestrian scramble
- Bicycle box
- Conflict zone markings for bicycles (e.g. coloured lane, skip lines, chevrons, sharrows)
- Audible pedestrian crossing signals
- Countdown signals
- Leading pedestrian and/or bicycle signals (advance walk/bike signal)
- Pedestrian and/or bicycle actuated signals
- Right-turn on red light prohibitions
- Mid-block signalized crossings

Traffic Calming

- In greenfield development, or where new streets are introduced through infill development, traffic calming will be achieved on neighbourhood streets by using:
 - Minimum traffic lane widths
 - Minimum number of traffic lanes in the roadway
 - Pedestrian-priority streets, woonerfs or home-zones (speed limit under 15 km/hr, vehicles must yield to pedestrians and cyclists)
- Through infill development, traffic calming should be achieved on existing neighbourhood streets by using any of, but not limited to, the following elements:
 - Reduced/minimum traffic lane width
 - Reduced/minimum number of traffic lanes in the roadway
 - Pedestrian-priority streets, woonerfs or home-zones (speed limit under 15 km/hr, vehicles must yield to pedestrians and cyclists)
 - Speed humps
 - Bollards (short vertical posts with shields)
 - Channelization islands (raised islands that force traffic to turn in a particular direction)
 - Chicane (curb bulges or planters or alternating sides, forcing motorists to slow down)

- On-street parking
- Choker (raised islands in parking zones that narrow a roadway)
- Curb extension, planter, or centreline traffic island that narrows traffic lanes
- Horizontal shift (a lane centreline that curves or shifts)
- Rumble or warning strip
- Semi-diverter or partial closure (restricts entry and limits traffic flow at intersections)
- Signal timing to reduce traffic speeds
- Radar trailer that shows drivers their current speed and the posted speed limit
- Traffic circles or roundabouts
- Speed table

- While increasing comfort and safety for pedestrians, the design of traffic calming elements should not create undue hazards or obstacles for cyclists.

Lighting

- All mixed-use streets must have an average luminance of 10 lux, with a minimum of 5 lux, or Town standard.
- Pedestrian-level street lamps of 4.6 metres in height or less, spaced apart no more than 30 metres, must be provided on all streets.

Wayfinding

- A wayfinding system should be implemented on a community-wide basis to allow residents and visitors to determine their location; identify key destinations (parks, transit stations, community and cultural facilities, shopping centres, off road trails); and develop a plan to take them from their location to desired destination by walking or cycling. The wayfinding system may include maps, directional signs or other elements, and should be useful and easy to understand.

Automobile Parking

- Reductions in parking requirements should be given to:
 - Buildings and other facilities within 400 metres of a transit stop; and,

- Apartments/condominiums offering car share parking spaces (with each car share space equivalent to 10 regular spaces).
- On-street parking should be included on all streets except where inappropriate for technical or safety reasons.
- Efficient use of parking should be promoted by identifying systems for sharing parking spaces by two or more user groups at different times of the day or week (for example, office staff use during weekdays and restaurant clientele in the evenings and on weekends), and by providing preferential parking for carpool vehicles.
- Where available, economic incentives should be identified and utilized to provide structured parking, rather than surface parking.
- Where surface parking is provided, it should be designed to minimize negative aesthetic and environmental impacts. This can be achieved by locating the parking lot way from the street frontage and by incorporating the following into the parking lot design:
 - Tree planting
 - Landscaping
 - Stormwater management
 - Porous/permeable surfaces
 - Light-coloured materials (rather than black asphalt)
 - Pedestrian access and circulation

Bicycle Parking

- All new developments should meet or exceed the higher of:
 - Local bicycle parking requirements (provided in local zoning by-laws, Bicycle Master Plans); or
 - The minimum bicycle parking standards outlined in Table 1.

Table 1. Minimum Bicycle Parking Standards, by Use and Type

Use	Minimum Space by Bicycle Parking Type	
	Occupant/ Employee*	Visitor **
Multi-unit Residential	0.7/unit	0.8/unit
Retail, Services & Community Facilities	0.1/100 m ²	3 + 0.25/100 m ²
General Office	0.15/100 m ²	3 + 0.25/100 m ²
Medical Office	0.15/100 m ²	3 + 0.1/100 m ²
Hospital	0.06/100 m ²	3 + 0.06/100 m ²
Elementary/ Secondary School	0.06/100 m ²	3 + 0.06/100 m ²
Post-Secondary School	0.06/100 m ²	3 + 0.2/100 m ²
Other non-residential (e.g. Industrial)	0.06/100 m ²	0.1/100 m ²
High-order Transit Station	<i>Complete a bicycle parking demand estimate for the station, for example using boardings, alightings and local bicycle mode share data.</i>	

*Occupant/Employee ("long-term") parking refers to secure, enclosed bicycle storage that is locked, weather protected and easily accessible to residents and/or workers. Signage indicating the location and information on use of these parking facilities should be provided.

**Visitor ("short-term") parking refers to outdoor, covered/uncovered bicycle racks.



On-street bicycle facilities

6.0 demonstration concept plans

The following illustrations are intended to depict a demonstration of the vision for the Downtowns of Fenwick, Fonthill and East Fonthill. The plans and before/after montages show just one scenario of the potential long-term build out of the downtowns that is consistent with the downtown urban design guidelines set out in this report. It should be noted that there may be other ways to achieve the same objectives.

1. New public plaza for community events and gatherings. Provides the Downtown Area with a clear focal point.

2. Potential gateway treatment to signal arrival to the Downtown Area, through the use of landscaping, signage, public art installations and/or building form.

3. Downtown improved streetscapes which potentially include widened sidewalks, street furniture, pedestrian lighting, and street trees.

4. Surface parking lots located to the rear of the property, and are concealed by landscaping.

5. On-street parking is provided as a buffer between pedestrians and vehicles, and acts as a traffic calming measure. It should be clearly demarcated from the sidewalk.

6. The new Library building and site is located directly in front the public plaza to reinforce civic uses.

7. At-grade retail uses address the sidewalk to ensure an active, continuous streetscape.

8. Special architectural features and public art are located at the terminus of view corridors.

6.1 Fenwick Demonstration Plan





Before | View looking south on Maple Avenue, towards Canboro Road. At present, the view terminus is the flagpole, post office, and Golden Grills Family Restaurant. A single-storey commercial building currently occupies the north-east corner at the intersection (left).



New buildings define the prominence of the corner, and retail uses like outdoor patios spill-out onto the sidewalk to create active pedestrian spaces.

New street trees and landscaped boulevards help animate the public realm, creating comfortable pedestrian environments for walking and biking.

Public landmarks are located at the terminus of view corridors.

After | Special paving, new landscaping along the boulevard, raised planting around the existing flagpole, as well as new street trees significantly improve the public realm for Downtown Fenwick. The single-store commercial building has been replaced with a more prominent building type, with spill-out activities to activate the sidewalk.

Existing character building with improved façade treatment using high-quality architectural materials and front-lit signage all contribute to a positive public realm.

Special paving highlight important intersections, help reduce vehicular travel speeds, and demarcate pedestrian crossings.

1. New proposed road to connect Highway 20 to Pelham Town Square.

2. Proposed network of rear lanes.

3. Peace Park is framed by building fronts and potential active spill-out uses like cafés and restaurants.

4. An opportunity for a new formal public plaza linking Pelham Street to Peace Park and where festivals and events such as farmer's market can be accommodated.

5. Potential gateway treatment to signal arrival to the Downtown Area, through landscaping, signage, public art installations and/or building form.

6. Mid-block connections and paths to enhance connectivity and promote walkability of the Downtown Area.

7. Downtown improved streetscapes.

8. Consolidated parking areas accessed by rear lanes.

9. Proposed new Town Hall / Library building with public parking underground.

10. At-grade retail uses address the sidewalk to ensure an active, continuous streetscape.

11. Special architectural features are located at the terminus of view corridors and key corner sites.

12. On-street parking is provided as a buffer between pedestrians and vehicles, and acts as a traffic calming measure. It should be clearly demarcated from the sidewalk.

13. Special paving at key intersections to help reduce vehicular travel speeds, and demarcate pedestrian crossings.

14. An attractive, high-quality shared intersection that provides flexibility for use of the space. Ability to be used as public space for festivals and events.

6.2 Historic Downtown Fonthill Demonstration Plan





Before | Pelham Town Square, view looking south of the current Town Hall (left) and free-standing commercial uses with surface parking area (center).



New Town Hall/Library building along the rear laneway. Retail uses located at-grade reinforce pedestrian activity so that the laneway doesn't just become a servicing lane.

The street becomes a new rear lane to access the Town Hall/Library (left) and the newly consolidated surface parking areas for mixed-use/commercial buildings that front onto Pelham Street (right).

New buildings with special architectural features are located at key visible sites.

Consolidated parking areas accessed by rear lanes.

After | View of new Town Hall/Library building along the laneway (left), envisioned with active retail uses at-grade. Special paving and bollards identify a clear pedestrian zone, and is buffered by raised planters which conceal the newly consolidated rear surface parking areas for the existing retail/mixed-use buildings that front onto Pelham Street (right).

Landscaped planters buffer the surface parking areas from the pedestrian walkway.

Special paving and bollards help reduce vehicular travel speeds and highlight pedestrian activity.



Before | View taken from Pelham Street, towards the entrance to the existing Town Hall. The corner sites are occupied by one-storey commercial/retail uses, and narrow sidewalks are located only along one-side of the street.



New buildings define the prominence of the corner. Active uses, like a small café, spill-out onto the sidewalk to provide year-round pedestrian activity.

New clock tower at the entrance into Peace Park becomes a clearly identifiable visual landmark in Fonthill, visible from Pelham Street.

New consolidated Town Hall/Library fronts onto Peace Park and frames the open space, and can be seen from Pelham Street.

After | A new pedestrian zone that at vehicular entrance to the Town Hall/Library provides space for a temporary farmer's market. Buried overhead wires, street trees, and a new, continuous pedestrian zone with plantings and special paving contribute to the newly envisioned public realm. In the foreground, two new three-storey corner buildings mark the pedestrian entrance to the Town Hall/Library. A new clock tower can be seen from Pelham Street, as a clearly identifiable landmark in Fonthill.

Special architectural features are located at the terminus of view corridors and key corner sites.

A new pedestrian zone provides key link between Pelham Street and Peace Park, where festivals and events such as the farmer's market can be accommodated.

7.0 implementation strategies

The Downtown Master Plan for Fenwick and Fonthill was informed by past and concurrent studies and initiatives, as well as a number of consultation events. It was tailored to the unique circumstances of Fonthill and Fenwick, while introducing ideas, benchmarks and standards rooted in good planning and urban design principles.

Downtowns, regardless of scale, are complex and unique parts of any community that require special attention to keep them operational, beautiful and successful. Downtowns do provide a number of challenges related to the risks and costs associated with ongoing redevelopment/evolution. Keeping a Downtown vital requires a municipality to focus their attention on a number of crucial “actions” that are intended to achieve positive and long-term results. Those crucial “actions” are typically identified as follows:

1. Establish the Environment for Change/Evolution; and,
2. Reduce the Risks and Costs of Development/Redevelopment.

7.1 Establish the Environment for Change/Evolution

A clear vision for the future - The Town, through the preparation of this Master Plan, has begun to establish the appropriate environment for change/evolution. The Master Plan provides a clear vision that is intended to guide future decision-making with respect to infrastructure emplacement, municipal investments and facilitating new development and redevelopment.

Ongoing public investment – Public investment in Downtowns, including investment from the federal, provincial, regional and local levels of government, is crucial. It sends a message to the private sector that the public sector is a willing partner in their success, because their success is fundamental to the image of the Town. Public investment in the public realm - parks, streetscapes and public buildings - will lead to a positive private sector development response, and will enhance the quality of life/quality of place that is experienced by the community and its visitors.

Political will – A broadly supported vision and the support of appropriate statutory planning documents will promote a strong political will to support the Downtowns. Long-term political will to implement the Master Plan is fundamental to a vital and constantly evolving Downtown. Political will must go beyond the four-year term of Council. Successful plans are implemented incrementally over a long period of time. Decisions today will have a tremendous impact on what the future will look like and feel like. Downtown improvement must be considered in terms of the legacy that will remain for future generations.

7.2 Reduce the Risks and Costs of Development/Redevelopment

A supportive planning policy framework – The Downtown Master Plan for Fenwick and Fonthill is a non-statutory plan, adopted by Council. As such, it can be used for guidance on matters of planning and financial investment. However, statutory force for matters of planning is only achieved through the implementing tools provided through the Planning Act. In Pelham, the relevant implementing tools include:

- *The Official Plan* – Policies are required that codify the vision, the land use and built form parameters and historic conservation requirements that are identified in the Master Plan must be included in the Official Plan to give the Master Plan necessary status. The Official Plan is also the over-arching statutory tool that can empower the municipality to prepare an implementing zoning by-law, utilize the Community Improvement legislation and require site plan approval control;
- *The Zoning By-Law* – In conformity with the policy framework of the Official Plan, the Zoning By-Law will establish the regulatory regime with respect to permitted land uses, the location of buildings on a lot, building height and density, as well as other regulations with respect to parking requirements, and building massing and articulation. Pre-zoning lands in these Downtowns for the anticipated scale of buildings and mixture of land uses reduces the risk of the development approval process, and provides all stakeholders with a sense of certainty when those important issues of development compatibility are discussed;

- *Site Plan Control* – *Site Plan Control* shall be required for all development/redevelopment in a Downtown context, except for residential uses with less than three dwelling units. *Site Plan Control* requires that detailed site planning is carried out to identify exactly how any development will fit into its site specific context. Specifically, items to be considered through the *Site Plan Control* process include:
 - The massing and conceptual design of the proposed building;
 - The relationship of the proposed building to adjacent buildings, streets, and exterior areas to which members of the public have access;
 - The provision of interior walkways, stairs, elevators and escalators to which members of the public have access from streets, open spaces and interior walkways in adjacent buildings;
 - Matters relating to exterior design, including without limitation the character, scale, appearance and design features of buildings, and their sustainable design;
 - The sustainable design elements on any adjoining roadway, including without limitation trees, shrubs, hedges, plantings or other ground cover, permeable paving materials, street furniture, curb ramps, waste and recycling containers and bicycle parking facilities; and,
 - Facilities designed to have regard for accessibility for persons with disabilities.
- The second type are incentives used to facilitate new development/redevelopment. These types of incentives can include grants/loans to promote new commercial or residential dwellings, waivers or reductions to Development Charges, waivers or reductions to planning application fees, and/or waivers for required engineering or environmental studies (where those studies have been completed on a larger scale).

Business Improvement Areas – BIA's provide an opportunity to enhance the partnership between the municipality and the local business community. They are a committee funded usually through an additional property tax levy, or a portion of the existing levy. The committee is charged with promoting the area to attract business, running events, providing enhanced maintenance and even funding or helping to fund, capital improvement projects. The key message is that a BIA is a collective voice for the business community who will work in partnership with Council to share costs and responsibilities for Downtown enhancement projects.

7.3 Ten Priority Items for Implementation

7.3.1 Amend the Official Plan and Zoning By-law

The Official Plan should be amended to include the Downtown Master Plan, including:

- The boundaries of the "Downtown" designation should be inclusive of the areas identified in the Built Form Framework Plan; and,
- Land use designations that enable the mixed-uses envisioned in the Built Form Framework.

The Zoning By-law should be area amended to be consistent with the built form categories (Built Form Framework Plan, Section 4.0), so as to implement the recommended standards with respect to:

- First, incentives to improve the character of existing buildings and landscapes. These types of incentives include grants/loans to facilitate façade improvements, landscape improvements and/or accessibility improvements; and,
- A mix of permitted land uses, including above-grade residential;

- Required retail at-grade along specified frontages;
- Minimum and maximum building heights;
- Minimum and maximum setbacks;
- Restricting parking area to the rear of the lot; and,
- Reduced parking standards appropriate for the “Downtown” context.

7.3.2 Prepare a Site Plan Control Manual

Site Plan Approval should be required for all developments within the “Downtowns” to ensure conformance with the Downtown Master Plan. The Town should utilize the Downtown Master Plan as the basis for new Site Plan Control Manual. The Manual will identify along with the traditional Site Plan requirements, colours, building materials and architectural detail (this will require empowering policy in the Official Plan). An updated Site Plan Control By-Law will be required to take full advantage of the Site Plan legislation provided in the new Planning Act, as recently amended by Bill 51.

7.3.3 Update Other Municipal Plans

The Transportation Plan should reflect the recommendations of the Built Form Framework for alterations to the road network and the introduction of lanes.

The Open Space Master Plan should include the proposed ‘Squares’ and ‘Plazas’ identified in the Public Realm Framework Plans, along with policies to secure them through parkland dedication requirements, or other means. The Open Space Plan should also prohibit the use of the alternative parkland dedication opportunity identified in the Planning Act within the “Downtowns”.

7.3.4 Implement Public Realm Improvements

The public realm includes streetscapes, parks and public buildings. The Town has already implemented a streetscape improvement in Downtown Fonthill, and is about to begin construction in Downtown Fenwick. Future public realm improvements to be considered as priorities include:

- Improvements to the Town Square Area, including Peace Park;
- A new Community Centre, parks and urban squares in the East Fonthill area;
- A “gateway” to Fonthill at the corner of Rice Road and Regional Road 20; and,
- A tree planting program in the rural sections of Canboro Road between Fenwick and Fonthill.

Other public realm improvements in Downtown Fenwick and Fonthill will be planned and implemented over time. The list included above will be reviewed on an annual basis, as part of the municipal budget.

7.3.5 Prepare a Detailed Town Square Master Plan

Consistent with the Fonthill Community Improvement Plan's, recommendation for a Master Plan for the Town Hall site, it is imperative that such a study be expanded to comprehensively explore the redesign of the entire Pelham Town Square area. The consideration for a new Town Hall either in the near or longer term provides for an opportunity to leverage a number of key area urban design objectives, including the re-configuration of the road network, framing of Peace Park and creation of new plaza linking the park to Pelham Street.

7.3.6 Prepare a Detailed Master Plan/Site Plan for the East Fonthill Mixed Use Area

The East Fonthill Mixed-Use Area provides an excellent opportunity to create a complementary anchor to Downtown Fonthill.

It is crucial that this area present a high quality public realm, including well designed stormwater management facilities, an interconnection Active Transportation system, parks, urban squares, beautiful streetscapes and potentially public buildings including a new community centre/area.

To facilitate this development, a detailed Master Plan/Site Plan has been prepared to inform implementing zoning and site plan approval.

7.3.7 Create a Design Review Process

To provide the Town with expertise urban design support, consider a Design Review process that involves an individual or panel of design professionals in a design advisory role. On an as-needed basis, the design advisor(s) can review and provide the Town with design recommendations for ensuring proposals meet the spirit and intent of the Downtown Master Plan. This process would be similar to the Architectural Control process that the Town has already employed for new development in Fonthill.

7.3.8 Expand the Community Improvement Plan Boundaries

Within Fonthill, consideration should be given to the expansion of the current Community Improvement Plan boundary to include the retail uses along Highway 20 and enlarged downtown transition areas, up to and including the Highway 20/Rice Road intersection. The purpose of this expansion is to begin to market the Fonthill retail community as an attractive and diverse regional shopping destination, and to provide a funding source to implement public realm capital improvement projects, joint marketing strategies and ongoing maintenance programs. The municipality should also ensure that matching funds are available from the Town to the BIA to implement capital projects, in conformity with

the Downtown Master Plan.

7.3.9 Establish BIA's in Downtown Fenwick and Fonthill

Establishment of Business Improvement Areas (BIA) for both Downtown Fenwick and Downtown Fonthill should be considered alongside incentive programs. The boundaries should align with the Community Improvement Plan boundaries, and in East Fonthill.

7.3.10 Update this Downtown Master Plan Regularly

Priorities and actions will change over time, and it is important that this Master Plan be updated to reflect new realities. It is recommended that this Downtown Master Plan be reviewed as part of the required 5-year review of the Official Plan, or alternatively, at an time where Council determines that the information and recommendations of this Plan are out of date.

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